

ROADWAY IMPROVEMENT PLANS

FOR

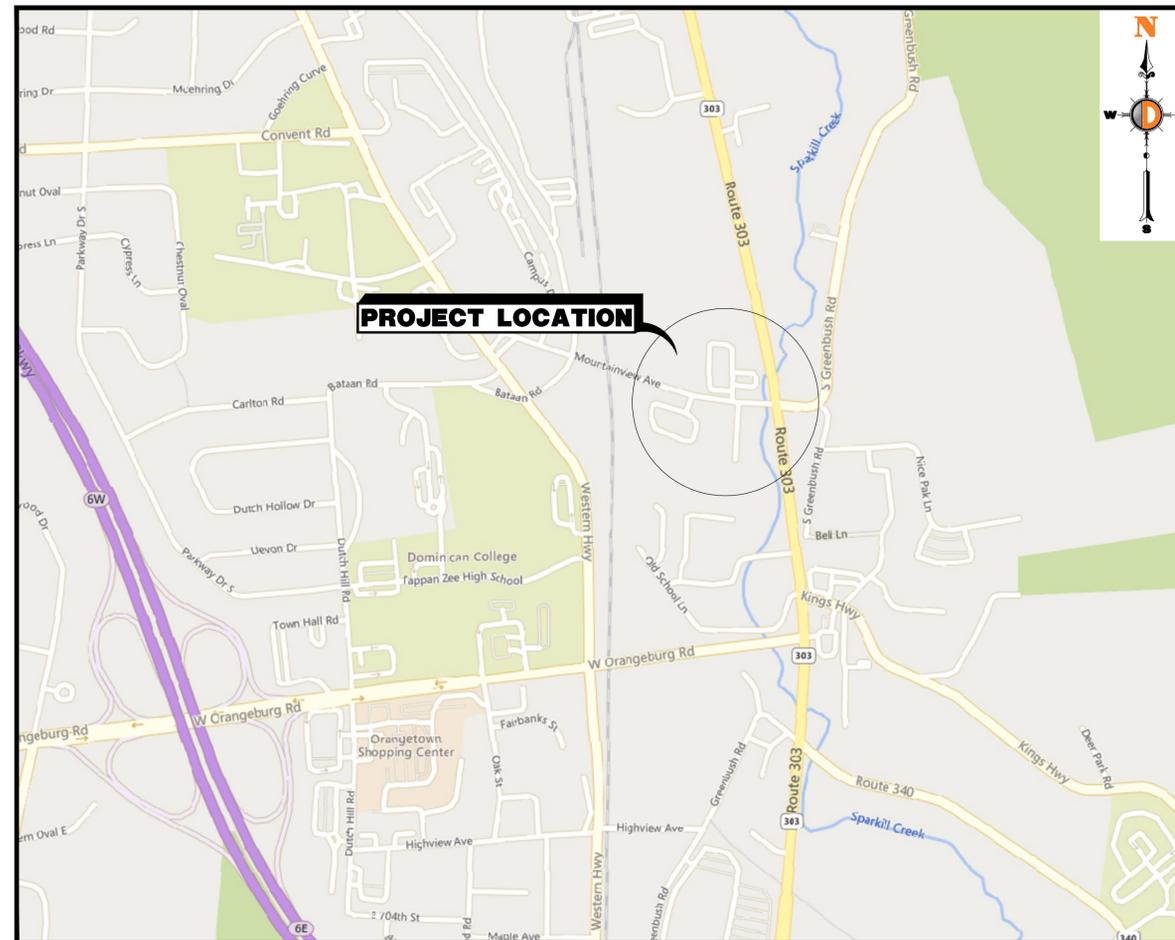
13 MOUNTAIN VIEW, LLC

TOWN OF ORANGETOWN

ROCKLAND COUNTY, NEW YORK

PUBLIC UTILITIES	
GAS/ELECTRIC	ORANGE AND ROCKLAND UTILITIES, INC. 390 WEST ROUTE 59 SPRING VALLEY, NY 10977
SANITARY SEWER	TOWN OF ORANGETOWN 127 ROUTE 303 ORANGETOWN, NY 10962
WATER	VEDUA WATER 162 OLD MILL ROAD WEST NYACK, NY 10994

NOTE: THE LOCATION OF ALL UNDERGROUND UTILITIES AS SHOWN HEREON ARE APPROXIMATE AND ARE BASED ON VISIBLE ABOVE GROUND STRUCTURES AND A UTILITY MARKOUT PROVIDED BY SURVEY AND ANY UTILITY MAPS PROVIDED BY UTILITY COMPANIES REFERENCED HEREON. NO EXCAVATIONS WERE MADE DURING THE PROGRESS OF SURVEY TO LOCATE BURIED UTILITIES/STRUCTURES. ADDITIONAL BURIED UTILITIES/STRUCTURES MAY BE ENCOUNTERED. THE CONTRACTOR SHALL CONTACT UTILITY COMPANIES 72 HOURS PRIOR TO CONSTRUCTION TO DETERMINE EXACT LOCATION AND DEPTH OF ALL UTILITIES. THE CONTRACTOR SHALL USE THE UTILITY LOCATIONS SHOWN AS AN AID IN DETERMINING EXACT LOCATIONS.



KEY MAP
1" = 500'

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STANDARD DETAILS AND SPECIFICATIONS TO BE USED

THE TOWN OF ORANGETOWN STREET SPECIFICATIONS FOR SUBDIVISIONS, THE ROCKLAND COUNTY STANDARD SPECIFICATIONS AND NEW YORK STATE DEPARTMENT OF TRANSPORTATION STANDARD SHEETS AND DETAILS SHALL GOVERN AS UPDATED AND AMENDED BY NYS DOT. THE CONTRACTOR SHALL BE RESPONSIBLE TO CONFIRM IF SUPPLEMENTARY SPECIFICATIONS ARE APPLICABLE.

Plotted: 05/15/25 - 10:23 AM, By: aaron, Product Ver: 25.0
 File: T:\TRAFFIC PROJECTS\3566 Link Industrial Management LLC\24-04790 Orangetown Dwg\Roadway\Draw\Roadway\135662404790K.dwg, ---> 01 KEY SHEET

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KEY SHEET
ROADWAY IMPROVEMENT PLANS
13 MOUNTAINVIEW AVENUE
SECTION 74.07 - BLOCK 1 - LOTS 33 & 36
TOWN OF ORANGETOWN, ROCKLAND COUNTY, NY

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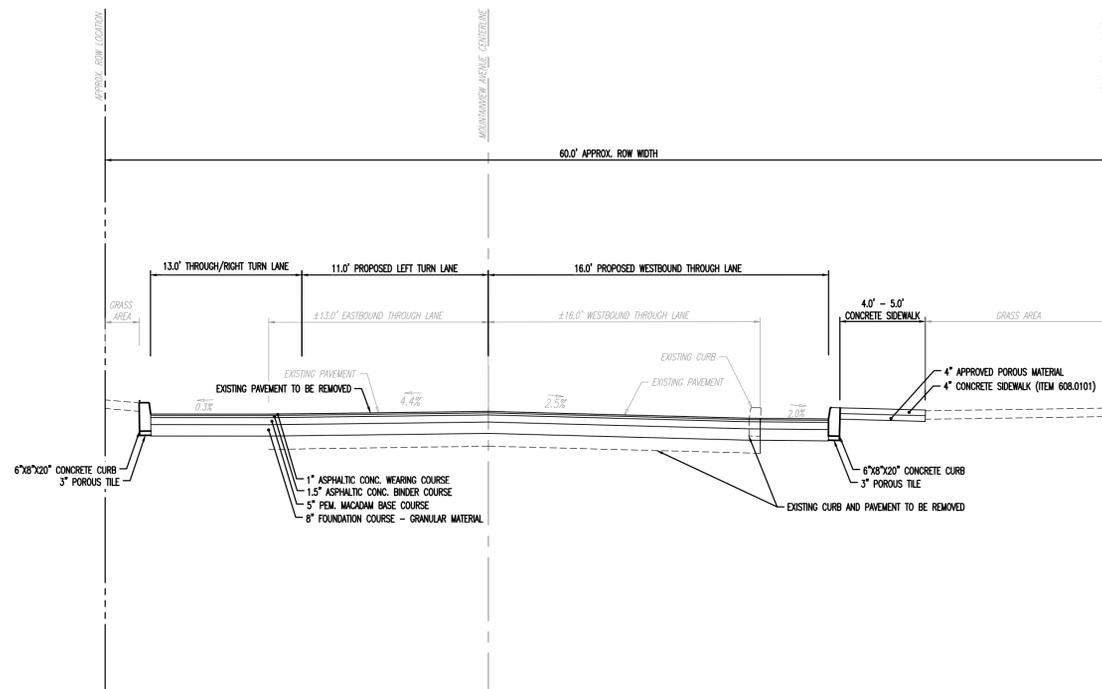
KEY OF NEW JERSEY
KEVIN M. SAVAGE
PROFESSIONAL ENGINEER
NEW YORK LICENSE No. 093631

COREY M. CHASE
PROFESSIONAL ENGINEER
NEW YORK LICENSE No. 093631

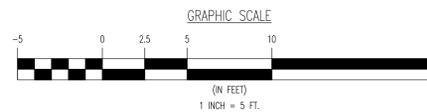
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PROJECT NUMBER: 3566 24-04790
SHEET NUMBER: 1 OF 11

DATE: 05/15/2025
REV. #: 0

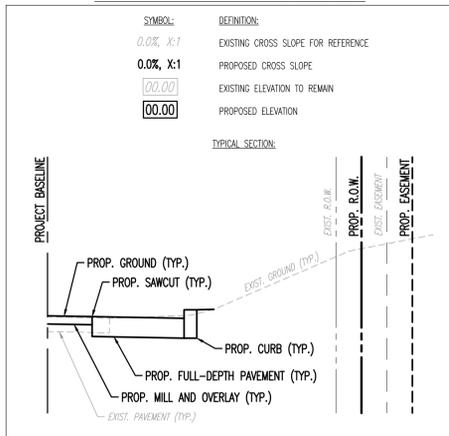
K-1



MOUNTAINVIEW AVENUE TYPICAL SECTION



TYPICAL SECTION LEGEND



NOTES:

1. THE EXISTING PAVEMENT DEPTH IS NOT KNOWN. THE EXISTING PAVEMENT DEPTH SHOWN IS FOR ILLUSTRATIVE PURPOSES ONLY.
2. REFER TO CONSTRUCTION DETAILS FOR COMMERCIAL STREET SPECIFICATIONS.

Plotted: 05/15/25 - 10:24 AM, By: afernante, Product Ver: 25.0
 File: T:\TRAFFIC PROJECTS\3566 Link Industrial Management LLC\24-04790 Orangetown\Drawings\Roadway\Drawings\35662404790\TYP.DWG, ---> 03 TYPICAL SECTIONS

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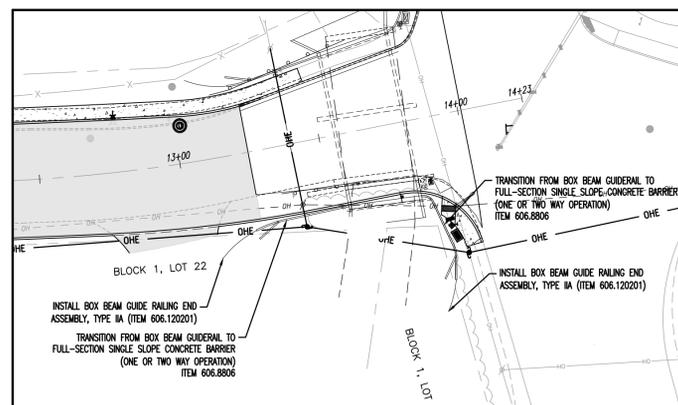
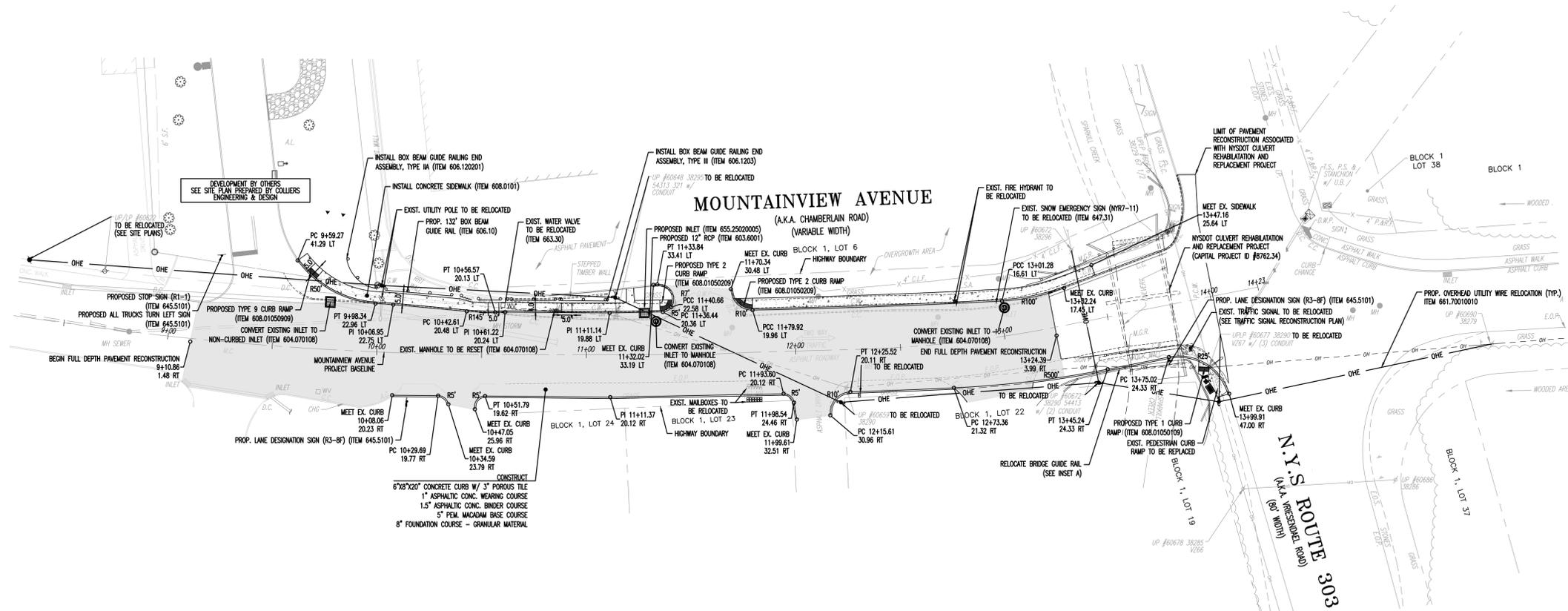
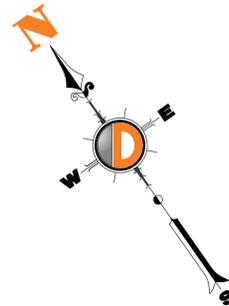
**TYPICAL SECTIONS
 ROADWAY IMPROVEMENT PLANS**
 13 MOUNTAINVIEW AVENUE
 SECTION 74.07 - BLOCK 1 - LOTS 33 & 36
 TOWN OF ORANGETOWN, ROCKLAND COUNTY, NY

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KEYES M. SAWAGE
 PROFESSIONAL ENGINEER
 NEW YORK LICENSE No. 093631

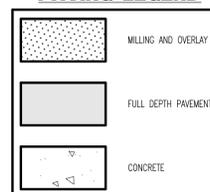
COREY M. CHASE
 PROFESSIONAL ENGINEER
 NEW YORK LICENSE No. 093631

SCALE: 1"=5'
 PROJECT NUMBER: 3566 24-04790
 DATE: 05/15/2025
 SHEET NUMBER: 3 OF 11



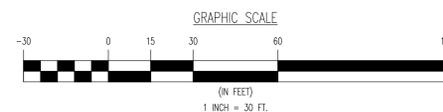
INSET A
SCALE: 1" = 30'

PAVING LEGEND



NOTES:

1. APPLY STRAIGHT TACK COAT (ITEM 407.0103) BETWEEN ALL PROPOSED PAVEMENT LAYERS.
2. APPLY ASPHALT PAVEMENT JOINT ADHESIVE (ITEM 418.7603) AT ALL SAWCUT LOCATIONS.
3. PROPOSED SAWCUT TO BE PLACED TO ALLOW FOR A MINIMUM 2 FT PAVEMENT SECTION IN FRONT OF PROPOSED CURB.
4. EXISTING INLETS TO BE INSPECTED BY CONTRACTOR AND REPLACED IF NECESSARY



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File: T:\TRAFFIC PROJECTS\3566 Link Industrial Management LLC\24-04790 Orangetown Dwg\Roadway Plans\35662404790\00.dwg, ----> 04 CONSTRUCTION PLAN

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**CONSTRUCTION PLAN
ROADWAY IMPROVEMENT PLANS**
 13 MOUNTAINVIEW AVENUE
 SECTION 74.07 - BLOCK 1 - LOTS 33 & 36
 TOWN OF ORANGETOWN, ROCKLAND COUNTY, NY

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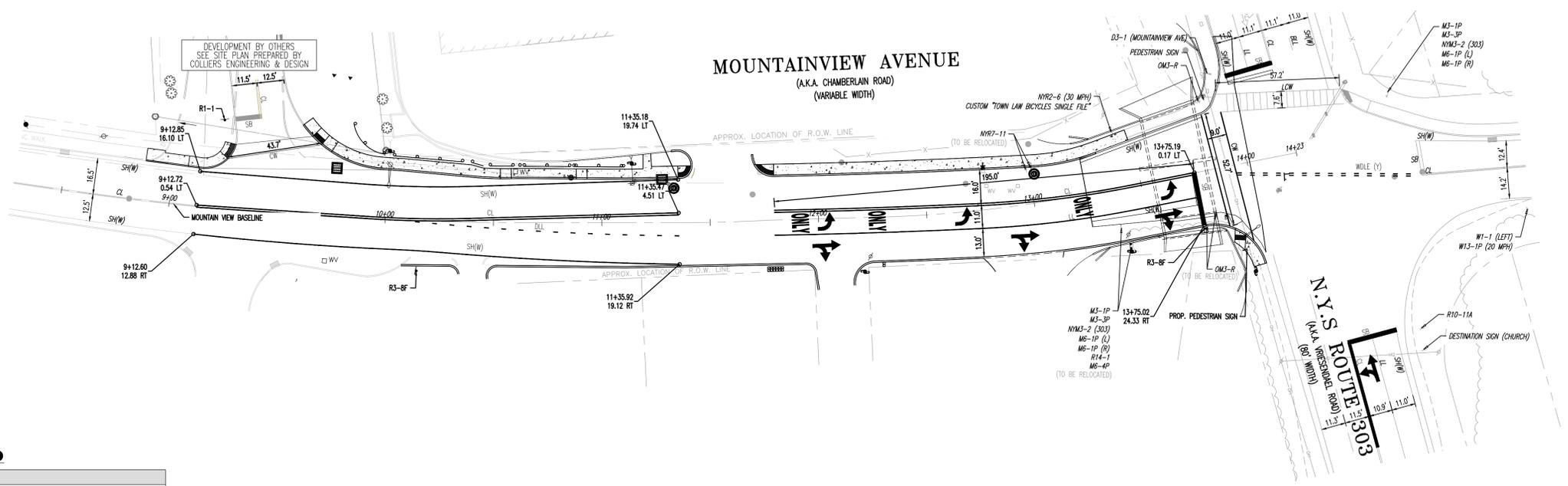
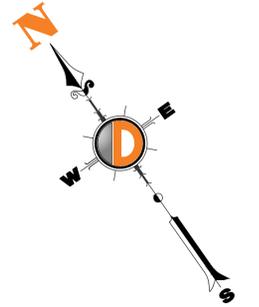
KEYVIN M. SAWAGE
 PROFESSIONAL ENGINEER
 NEW YORK LICENSE NO. 09353

COREY M. CHASE
 PROFESSIONAL ENGINEER
 NEW YORK LICENSE No. 093531

SCALE: 1"=30'
 DRAWN BY: ARF
 DESIGNED BY: ARF
 CHECKED BY: KMS

PROJECT NUMBER: 3566 24-04790
 DATE: 05/15/2025
 REV. #: 0

SHEET NUMBER:
4 OF **11**



PAVEMENT MARKING LEGEND

ABBREVIATION	PAVEMENT MARKING	MATERIAL	DESCRIPTION
SB	STOP BAR	THERMOPLASTIC	24" SOLID WHITE
YM	YIELD MARKINGS	THERMOPLASTIC	24" WIDE X 36" LONG WHITE TRIANGLES SPACED AT 36" O.C.
CL	CENTER LINE	EPOXY RESIN	TWO 4" SOLID YELLOW LINES, SPACED 10" O.C.
CLE	CENTER LINE EXTENSION	EPOXY RESIN	4" SOLID YELLOW, 2' LONG WITH 6" GAPS
SH(W)	SHOULDER LINE (WHITE)	EPOXY RESIN	4" SOLID WHITE
SH(Y)	SHOULDER LINE (YELLOW)	EPOXY RESIN	4" SOLID YELLOW
LL	LANE LINE	EPOXY RESIN	4" SOLID WHITE
WLL	WIDE LANE LINE	EPOXY RESIN	6" SOLID WHITE
BL	BROKEN LANE LINE	EPOXY RESIN	4" SOLID WHITE, 10' LONG WITH 30' GAPS
WBL	WIDE BROKEN LANE LINE	EPOXY RESIN	6" SOLID WHITE, 10' LONG WITH 30' GAPS
DLE	DOTTED LANE EXTENSION	EPOXY RESIN	4" SOLID WHITE, 2' LONG WITH 6" GAPS
WDL	WIDE DOTTED LANE EXTENSION	EPOXY RESIN	6" SOLID WHITE, 2' LONG WITH 6" GAPS
DL	DOTTED LANE LINE	EPOXY RESIN	4" SOLID WHITE, 3' LONG WITH 9" GAPS
WDL	WIDE DOTTED LANE LINE	EPOXY RESIN	6" SOLID WHITE, 3' LONG WITH 9" GAPS
CW	CROSSWALK	THERMOPLASTIC	6" SOLID WHITE
CCW	CONTINENTAL CROSSWALK	THERMOPLASTIC	24" SOLID WHITE TRANSVERSE LINES, SPACED AT 4' O.C.
LOW	LADDER CROSSWALK	THERMOPLASTIC	6" SOLID WHITE LONGITUDINAL LINES AND 24" SOLID WHITE TRANSVERSE LINES, SPACED AT 4' O.C.
CH(W)	CROSSHATCH MARKINGS (WHITE)	THERMOPLASTIC	24" WHITE LINES, ORIENTED AT 45 DEGREES, SPACED AT 12" O.C.
CH(Y)	CROSSHATCH MARKINGS (YELLOW)	THERMOPLASTIC	24" YELLOW LINES, ORIENTED AT 45 DEGREES, SPACED AT 12" O.C.
DNBI	DO NOT BLOCK INTERSECTION MARKINGS	EPOXY RESIN	12" WHITE BORDER WITH 6" WHITE CROSS-HATCH LINES, ORIENTED AT 45 DEGREES, SPACED AT 12" O.C.

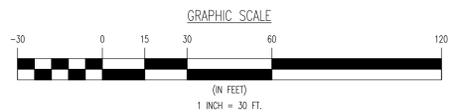
- NOTES:
- ALL TRAFFIC CONTROL DEVICES SHALL BE IN CONFORMANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.).
 - THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING EXISTING SIGNING AND PAVEMENT MARKINGS WITHIN THE PROJECT LIMITS DURING CONSTRUCTION, UNLESS OTHERWISE SHOWN ON THE PLANS.
 - EXISTING STRIPING THAT DOES NOT CONFORM TO THIS PLAN SHALL BE REMOVED BY THE HYDROBLASTING METHOD. FOR ANY EXISTING WORD MARKINGS TO BE REMOVED, THE HYDROBLASTING SHALL BE PERFORMED ON AN ENTIRE RECTANGULAR SHAPE OF PAVEMENT ENCOMPASSING THE ENTIRE WORD RATHER THAN HYDROBLASTING THE INDIVIDUAL LETTER SHAPES.
 - EXISTING PAVEMENT MARKINGS THAT ARE TO REMAIN WITHIN THE LIMITS OF WORK SHALL BE RESTRIPE IN KIND IF THEY ARE FOUND TO BE WORN OR FADED. THE DECISION TO RESTRIPE EXISTING MARKINGS TO REMAIN SHALL BE AT THE DISCRETION OF THE ENGINEER.
 - A MINIMUM 4' CLEAR AREA SHALL BE PROVIDED BETWEEN THE NEAREST EDGES OF CROSSWALK AND STOP BAR AND/OR YIELD MARKINGS.
 - CROSSWALKS SHALL BE A MINIMUM OF 6' WIDE. IF LONGITUDINAL LINES ARE PROVIDED, A MINIMUM OF 6' CLEAR SPACE SHALL BE PROVIDED BETWEEN THE INSIDE EDGES OF THE LONGITUDINAL LINES.
 - WORD AND ARROW SYMBOL MARKINGS SHALL BE THERMOPLASTIC.
 - SHOULDER LINES ADJACENT TO THE EDGE OF THE TRAVELED WAY (CURB OR EDGE OF PAVEMENT) SHALL BE LOCATED 1' FROM THE EDGE OF THE TRAVELED WAY UNLESS OTHERWISE NOTED.
 - AT ALL TIMES DURING CONSTRUCTION DURING WHICH ROADWAYS ARE OPEN TO PUBLIC TRAFFIC, THE CONTRACTOR SHALL ENSURE THAT THE CONFIGURATION OF TRAFFIC CONTROL DEVICES PROVIDED IS COMPLIANT WITH THE M.U.T.C.D.

EXISTING SIGN LEGEND

MUTCD DESIGNATION	DESCRIPTION
NM3-2	NY STATE ROUTE (NYS ROUTE 303)
NYR2-6	TOWN SPEED LIMIT (30 MPH)
NYR7-11	SNOW EMERGENCY ROUTE
M3-1P	CARDINAL DIRECTION (NORTH)
M3-3P	CARDINAL DIRECTION (SOUTH)
M6-1P(L)	DIRECTIONAL ARROW (LEFT)
M6-1P(R)	DIRECTIONAL ARROW (RIGHT)
M6-4P	DIRECTIONAL ARROW (LEFT/RIGHT)
R1-1	STOP
R10-11A	NO TURN ON RED
R14-1	TRUCK ROUTE
D3-1	STREET NAME ("MOUNTAINVIEW AVE")
OM3-R	OBJECT MARKER
W1-1	HORIZONTAL ALIGNMENT (LEFT)
W13-1P	ADVISORY SPEED PLAQUE (20 MPH)

PROPOSED SIGN LEGEND

MUTCD DESIGNATION	DESCRIPTION
R3-8F	LANE DESIGNATION



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 NEW YORK LICENSE No. 093631

SCALE: 1"=30'
 DRAWN BY: CJM
 DESIGNED BY: ARF
 CHECKED BY: KMS

PROJECT NUMBER: 3566-24-04790
 DATE: 05/15/2025
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SHEET NUMBER: **6** OF **11**

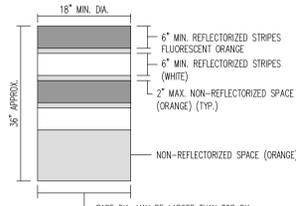
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 File: T:\TRAFFIC PROJECTS\3566 Link Industrial Management LLC\24-04790 Orangeburg Dwg\NYSOT Traffic Control Plan\35662404790TPO.dwg, ---> 07 TRAFFIC CONTROL NOTES AND DETAILS

ENSURE DRUMS ARE MADE OF ORANGE PLASTIC WITH A MINIMUM OF FOUR ALTERNATE FLUORESCENT ORANGE AND WHITE RETROREFLECTIVE STRIPES. IF THERE ARE NON-REFLECTORIZED SPACES BETWEEN THE STRIPES, THEY SHALL BE NO MORE THAN 2" WIDE. ENSURE RETROREFLECTIVE SHEETING FOR STRIPES CONFORMS WITH ASTM D 4956 TYPE VII OR VIII WITH S2 REQUIREMENTS.

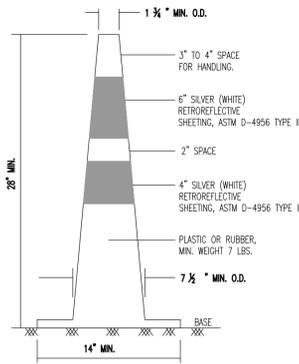
ENSURE THE TOP OF THE DRUM IS NOT OPEN. CONSTRUCT DRUMS TO INHIBIT ROLLING IF KNOCKED OVER.

ENSURE THE REFLECTORIZED AREA OF DRUMS SHALL BE ROUND EXCEPT THAT OTHER SHAPES, WHICH PROVIDE THE SAME VISIBILITY AS AN 18 INCH DIAMETER ROUND DRUM REGARDLESS OF ORIENTATION.



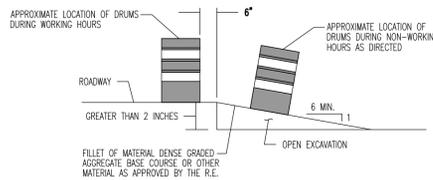
WHEN BALLAST IS REQUIRED BY THE R.E. USE SAND. THE MAXIMUM WEIGHT OF THE BALLAST SHALL BE 50 LBS. AND IS TO BE LOCATED APPROXIMATELY AT GROUND LEVEL. ALTERNATE TYPES OF BALLAST SHALL BE APPROVED BY THE R.E. (FOR USE WITH NJDOT ROW ONLY)

TRAFFIC DRUM DETAIL
NOT TO SCALE



NOTES:
 1. TRAFFIC CONES SHALL BE PREDOMINATELY ORANGE IN COLOR.
 2. BASES MAY BE OF BREAKAWAY BALLASTED TYPE.
 3. MINOR MANUFACTURER'S VARIATIONS MAY BE ACCEPTABLE UPON APPROVAL OF THE ENGINEER.

TRAFFIC CONE DETAIL
NOT TO SCALE



NOTE:
ESCAPE RAMP MUST BE CONSTRUCTED AND MAINTAINED DURING NON-WORKING HOURS WHERE A VERTICAL DROP GREATER THAN 2 INCHES EXISTS ADJACENT TO TRAVELED LANE.

ESCAPE RAMP DETAIL
NOT TO SCALE

LEGEND:

- ARROW PANEL
- ARROW PANEL, CAUTION MODE
- ARROW PANEL TRAILER OR SUPPORT
- CHANGEABLE MESSAGE SIGN (PMS)
- CHANNELIZING DEVICE
- CRASH CUSHION/TEMPORARY IMPACT ATTENUATOR
- DIRECTION OF TEMPORARY TRAFFIC DETOUR
- DIRECTION OF TRAFFIC
- FLAGGER
- FLAG TREE
- LUMINAIRE
- PAVEMENT MARKINGS THAT SHALL BE REMOVED FOR A LONG TERM PROJECT
- SIGN, TEMPORARY
- TEMPORARY BARRIER
- TEMPORARY BARRIER WITH WARNING LIGHTS
- TRAFFIC OR PEDESTRIAN SIGNAL
- TYPE III BARRICADE
- WARNING LIGHTS
- WORK SPACE
- WORK VEHICLE
- WORK VEHICLE W/ TRUCK MOUNTED ATTENUATOR
- PROPOSED LIGHTING FIXTURE
- PROPOSED BACK TO BACK LIGHTING FIXTURE

LEGEND

NOT TO SCALE

WORK ZONE TRAFFIC CONTROL NOTES

1. ALL WORK ZONE TRAFFIC CONTROL SHALL CONFORM TO THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), AS WELL AS THE NYSOT MUTCD SUPPLEMENT AND STANDARD SPECIFICATIONS, SECTION 619, EXCEPT AS MODIFIED IN THE PLANS AND PROPOSAL. CLOSURES MUST BE APPROVED BY THE HUDSON VALLEY TRANSPORTATION MANAGEMENT CENTER (TMC).
2. IF AT ANY TIME, IN THE OPINION OF THE ENGINEER, CONDITIONS WARRANT MODIFICATIONS TO THE SCHEMES SHOWN ON THIS OR OTHER MAINTENANCE AND PROTECTION OF TRAFFIC DRAWINGS, THE CONTRACTOR SHALL PERFORM THE MODIFICATIONS INCLUDING RE-OPENING OF ANY LANE CLOSURES ON AN EMERGENCY BASIS A.O.B.E.
3. CONSTRUCTION SIGNS SHALL BE POSTED ONLY WHEN WORK IS IN PROGRESS. CONSTRUCTION SIGNS HAVING CENTER HINGED SIGN PANELS OR HAVING FOLDING PORTABLE SIGN SUPPORTS SHALL BE FOLDED DOWN WHEN THE WORK THEY PERTAIN TO IS NOT IN PROGRESS. OTHER CONSTRUCTION SIGNS WHICH CANNOT BE FOLDED DOWN SHALL BE REMOVED OR COVERED. THE COVERING OF SIGNS SHALL CONFORM TO THE REQUIREMENTS OF SECTIONS 619-2.02H AND 645-3.09 OF THE NYSOT STANDARD SPECIFICATIONS.
4. ALL CONSTRUCTION SIGNS SHALL HAVE AN ORANGE BACKGROUND AND BLACK LETTERS AND BORDERS (REFLECTORIZED IS USED AT NIGHT). CORNERS OF SIGNS SHALL NOT BE CUT. THE CONTRACTOR SHALL IDENTIFY ALL CONSTRUCTION SIGNS WITH "CONTRACTOR'S NAME, CONTRACT NO. (D...), NYSOT" WRITTEN ON THE BACK OF THE SIGN. THE LETTERS SHALL BE 3 IN. HIGH. THE CONTRACTOR IS TO SUBMIT THE TEXT AND SIZE OF CONSTRUCTION SIGNS TO THE E.I.C. BEFORE FABRICATION.
5. THE BOTTOM OF TEMPORARY CONSTRUCTION SIGNS SHALL BE A MINIMUM OF 7 FT ABOVE THE TRAVEL PAVEMENT ON EXPRESSWAYS AND PARKWAYS AND THE EDGE OF SIGNS A MINIMUM OF 2 FT CLEAR OF THE TRAVEL LANE, AS SHOWN ON THE CONTRACT DOCUMENTS OF A.O.B.E. IF THE 2 FT HORIZONTAL CLEARANCE CANNOT BE MET, THE CONTRACTOR MAY USE THE EQUIVALENT RECTANGULAR SIGN, IF APPROVED BY THE E.I.C. UNDER NO CIRCUMSTANCES WILL THE CLIPPING OF SIGN OR USAGE OF PREVIOUSLY CLIPPED SIGNS BE ALLOWED.
6. THE LOCATION OF CONSTRUCTION SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL (WZTC) PLANS ARE APPROXIMATE ONLY. THE EXACT LOCATIONS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD. TEMPORARY SIGNS SHALL NOT BE PLACED AT ANY LOCATION WHERE THEY ARE OBSCURED BY TEMPORARY OR PERMANENT OBJECTS.
7. WARNING SIGNS SHALL BE LOCATED TO PROVIDE ADEQUATE VISIBILITY DISTANCE TO DRIVERS. THEY SHALL NOT BE BLOCKED BY FOLIAGE, ROADWAY FEATURES, OR OTHER SIGNS AND TRAFFIC CONTROL DEVICES. NO STATIONARY MOUNTING OF CONSTRUCTION SIGNS SHALL BE PERMITTED ON UTILITY POLES OR OTHER ROADSIDE ELEMENTS. FOR NIGHTTIME CONSTRUCTION, SIGN PLACEMENT SHALL ALSO CONSIDER CLARE FROM LIGHT SOURCES LOCATED BEHIND THE SIGN AND LOW BEAM HEADLIGHT PATTERNS. REFLECTORIZED SIGNS SHALL BE ORIENTED ESSENTIALLY PERPENDICULAR TO THE DIRECTION OF TRAFFIC. WHERE THE WARNING SIGN CAN NOT BE LOCATED TO BE VISIBLE UNDER HEADLIGHT ILLUMINATION, OTHER ILLUMINATION SHALL BE CONSIDERED TO ENHANCE VISIBILITY OR THE SIGN SHALL BE MOVED.
8. IN REFERENCE TO THE NYSOT MUTCD SUPPLEMENT AND STANDARD SPECIFICATIONS, SECTION 619, THE FOLLOWING STIPULATIONS SHALL APPLY UNLESS OTHERWISE SPECIFIED BY THE ENGINEER:
 - A. WHERE SIGNS ARE SHOWN IN BOTH DIAMOND AND RECTANGULAR SHAPES, ONLY DIAMOND SHAPES SHALL BE PERMITTED, OR A.O.B.E.
 - B. WHERE SIGNS ARE SHOWN IN ALTERNATE SIZES, THE LARGEST SIZE MUST BE USED UNLESS OTHERWISE SPECIFIED BY THE ENGINEER, OR AS SHOWN IN THE PLANS.
9. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO FURNISH, INSTALL, MAINTAIN AND REMOVE ALL SIGNS AND TRAFFIC CONTROL SAFETY DEVICES.
10. EXISTING GUIDE, WARNING AND REGULATORY SIGN MESSAGES THAT ARE RENDERED INAPPLICABLE BY CONSTRUCTION ACTIVITIES SHALL BE COVERED FOR THE DURATION OF WORK IN PROGRESS.
11. UNDER THE BASIC WZTC ITEM, THE CONTRACTOR SHALL BE REQUIRED TO PERFORM MAINTENANCE CLEANING OF THE PAVEMENT AND SHOULDER AREAS WITHIN THE CONTRACT LIMITS WHEN ORDERED BY THE ENGINEER. MAINTENANCE CLEANING SHALL MEAN THE REMOVAL OF DEBRIS FROM ANY SOURCE, WHICH IN THE OPINION OF THE ENGINEER IMPEDS FLOW OF TRAFFIC OR STORM WATER. THIS REQUIREMENT SHALL NOT BE CONSTRUED TO CHANGE THE PROVISIONS OF ARTICLE 619-1.02K SNOW AND ICE CONTROL OF THE NYSOT STANDARD SPECIFICATIONS.
12. TRAVEL LANES SHALL BE SWEEPED CLEAN BY THE CONTRACTOR BEFORE THEY ARE RE-OPENED TO TRAFFIC. ALL PAVEMENT LANE MARKINGS SHALL BE MAINTAINED AND RESTORED AFTER COMPLETION OF WORK AND SHALL BE PAID FOR UNDER ITEM 619.01.
13. TO ENSURE A SAFE TRAFFIC FLOW AT ALL TIMES, STORAGE OF MATERIALS AND EQUIPMENT, INCLUDING EMPLOYEES CARS, SHALL NOT BE PERMITTED WITHIN THE TRAVELED WAY OF HIGHWAY. STORAGE AREAS SHALL BE SEPARATED FROM THE TRAVELED WAY BY A CLEAR SPACE OF 30 FT MINIMUM WIDTH, UNLESS SUCH STORAGE IS PLACED BEHIND TEMPORARY CONCRETE BARRIER OR OTHER PERMANENT ROADSIDE BARRIER INSTALLATION.
14. THE WZTC SCHEMES SHOWN IN THE PLANS ARE TO PROTECT THE TRAVELING PUBLIC. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT THE WORKERS AND TO PROVIDE THEM WITH SAFE ACCESS TO WORK SITES.
15. LANE CLOSURES SHALL BE STARTED TO PROVIDE OPTIMUM VISIBILITY, I.E., BEFORE CURVES AND CRESTS AND SHOULD BE LOCATED AWAY FROM OTHER CONFLICT POINTS SUCH AS ON-RAMPS AND INTERSECTIONS WHENEVER POSSIBLE. ALSO, CLOSURES SHALL BE STARTED SO AS TO AVOID BRIDGES AND UNDERPASSES AND OTHER LOCATIONS WITHOUT AVAILABLE ESCAPE PATHS.
16. WHEN WORKING ON THE MEDIAN WITH ONLY ONE SPEED LANE CLOSED, THE CONTRACTOR SHALL PLACE CONES ALONG THE GUTTER LINE OF THE OPENED SPEED LANE SPACED AT 40 FT INTERVALS FOR THE ENTIRE LENGTH OF THE WORK ZONE.
17. THE CONCURRENT CLOSURES OF LEFT AND RIGHT LANES SHALL NOT BE PERMITTED WITHIN A 2 MILES DISTANCE BETWEEN CONSECUTIVE CLOSURES IN THE SAME TRAVEL DIRECTION, EXCEPT AS MODIFIED BY SPECIAL NOTES IN THE PROPOSAL.
18. AT LEAST ONE WEEK IN ADVANCE, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER, A PROPOSED LANE CLOSURE SCHEDULE. THE SCHEDULE SHALL INCLUDE LAYOUTS AND SIGNING AS SPECIFIED IN THE M&PT SHEETS AND OTHER METHODS AVAILABLE TO IMPROVE TRAFFIC FLOW THAT ARE NOT SHOWN ON THE M&PT SHEETS. THE SCHEDULE SHALL BE REVIEWED AND APPROVED BY THE ENGINEER BEFORE IMPLEMENTATION. A COPY OF THE APPROVED SCHEDULE SHALL BE FORWARDED TO N.Y.C.D.O.T. THE CONTRACTOR SHALL PERFORM THIS WORK IN SUCH A MANNER AND SEQUENCE AS TO MAINTAIN THE THROUGH TRAFFIC AS SHOWN IN THE PLANS AND PROPOSAL.
19. THE CONTRACTOR SHALL PROVIDE A FLAGGER WITH APPROPRIATE SIGNING WHENEVER OPERATIONS INTERFERE WITH TRAFFIC. EXAMPLES INCLUDE, BUT ARE NOT LIMITED TO, DELIVERY/REMOVAL OF MATERIALS, LIFTING OPERATIONS AND OTHER ACTIVITIES A.O.B.E. COST TO BE INCLUDED IN ITEM 619.01M. ALL FLAGGERS USED IN NIGHTTIME CONSTRUCTION SHALL BE TRAINED FORMALLY IN FLAGGING OPERATIONS.

WORK ZONE TRAFFIC CONTROL NOTES, CONT'D.

20. THE SOLE DUTY OF THE FLAGGER SHALL BE TO DIRECT TRAFFIC PROPERLY AT ALL TIMES. FLAGGERS SHALL NOT BE USED TO MOVE TEMPORARY SIGNS OR ASSIST IN OTHER WORK. REQUIREMENTS OF THE NATIONAL MUTCD AND NYSOT MUTCD SUPPLEMENT FOR HAND SIGNALING DEVICES SHALL BE MET. STOP/SLOW PADDLES SHALL BE USED RATHER THAN FLAGS FOR MOST LONG TERM FLAGGING OPERATIONS. STANDARD SIGNALING PROCEDURES SHALL BE FOLLOWED AS PER NATIONAL MUTCD AND NYSOT MUTCD SUPPLEMENT STANDARDS. CLEAN WELL-FITTING ORANGE VESTS AND HARD HATS SHALL BE WORN BY FLAGGERS. FLAGGING STATIONS SHALL BE LOCATED TO PROVIDE ADEQUATE SIGHT DISTANCE AND VISIBILITY. ADEQUATE ESCAPE PATH SHALL BE PROVIDED TO THE EXTENT POSSIBLE FOR THE SAFETY OF FLAGGERS AND FOR THE SAFE RECOVERY OF DRIVERS WHO FAIL TO RESPOND TO FLAGGER DIRECTIONS. ANY FLAGGING DEFICIENCIES SHALL BE CORRECTED IMMEDIATELY, OR THE CONTRACTOR SHALL BE REQUIRED TO SHUT DOWN HIS OPERATIONS UNTIL A COMPETENT FLAGGER IS OBTAINED.
21. IN THE EVENT THAT AN EXISTING TRAFFIC SIGNAL IS TURNED OFF A MINIMUM OF TWO FLAGGERS ARE REQUIRED AT ANY INTERSECTIONS TO DIRECT THE FLOW OF TRAFFIC. THIS IS TO BE DETERMINED BY THE ENGINEER.
22. PARKING METERS IN WORK ZONES SHALL BE COVERED AND SIGNED AS "NO PARKING".
23. FOR WORK ZONE SPEED REDUCTIONS IMPLEMENTED DURING CONSTRUCTION, IT SHALL BE THE RESPONSIBILITY OF THE REGIONAL CONSTRUCTION GROUP TO SELECT AN APPROPRIATE REDUCED SPEED THROUGHOUT THE WORK ZONE. REVIEW AND APPROVAL BY THE REGIONAL CONSTRUCTION ENGINEER AND THE TRAFFIC ENGINEER IS REQUIRED. THE DESIGN ENGINEER SHALL ALSO BE CONSULTED.
24. THE LANE CLOSURE SCHEMES HEREIN SPECIFIED ARE TO BE UTILIZED FOR DAYTIME APPLICATIONS. FOR LONG TERM OR OVERNIGHT OPERATIONS, THE CONTRACTOR SHALL USE DRUMS IN PLACE OF ALL CONES. THE FIRST TWO CHANNELIZING DEVICES AT THE BEGINNING OF LANE CLOSURES AND SHOULDER CLOSURES SHALL BE FITTED WITH WARNING LIGHTS IN ACCORDANCE WITH NYSOT MUTCD SUPPLEMENT SECTION 294.3. IN ADDITION, THE CONTRACTOR SHALL OBSERVE THE FOLLOWING:
 - A. TYPE A (LOW INTENSITY) OR TYPE B (HIGH INTENSITY) FLASHING WARNING LIGHTS ARE TO BE USED AS THE FIRST TWO LIGHTS IN A LONGITUDINAL SERIES AND FOR MARKING ISOLATED HAZARDS.
 - B. TYPE C (LOW INTENSITY) STEADY BURNING WARNING LIGHTS MAY ONLY BE USED ON TEMPORARY CONCRETE BARRIER OR OTHER NON REFLECTIVE FEATURES LOCATED CLOSE TO TRAVEL LANES. THEY ARE NOT TO BE USED ON REFLECTORIZED CHANNELIZING DEVICES UNLESS JUSTIFIED BY REDUCED VISIBILITY OR HEAVY FOG.
 - C. TYPE A AND TYPE C SHALL BE USED FOR NIGHTTIME APPLICATIONS. TYPE B SHALL BE USED FOR DAYTIME APPLICATIONS, AND IT MAY BE USED AT NIGHT WHERE THE CHANNELIZING DEVICES ARE LEFT IN PLACE DURING DAYTIME AND WHERE ADDITIONAL EMPHASIS IS DESIRABLE.
25. FOR WZTC DURING NIGHTTIME OPERATIONS, WORK SPECIFICALLY SCHEDULED TO OCCUR AFTER SUNSET AND BEFORE SUNRISE, THE CONTRACTOR SHALL FOLLOW THE REQUIREMENTS OF SECTIONS 619-1.15 AND 619-3.19 OF THE STANDARD SPECIFICATIONS AND ITS ADDENDA.
26. FOR NIGHTTIME OPERATIONS, THE SPACING OF CHANNELIZING DEVICES FOR LANE CLOSURE TAPERS AND ADDENDA SHALL BE IN ACCORDANCE WITH SECTION 619-3.01J OF THE STANDARD SPECIFICATIONS AND ADDENDA.
27. DURING NIGHT TIME CONSTRUCTION, ALL CHANNELIZING DEVICES SHALL BE EQUIPPED WITH REFLECTIVE SHEETING MEETING THE REQUIREMENTS OF THE N.Y.S.D.O.T. STANDARD SPECIFICATIONS AND N.Y.S.M.U.T.C.D.
28. FOR NIGHTTIME OPERATIONS, THE LANE CLOSURES SHALL BE REINFORCED BY THE PLACEMENT OF TWO DRUMS IN THE CENTER OF CLOSED LANES. THE TWO DRUMS SHALL BE PLACED TRANSVERSELY ACROSS THE LANE WHILE PERMITTING PASSAGE OF CONSTRUCTION TRAFFIC. THEY MAY BE TEMPORARILY OMITTED, WHERE REQUIRED, TO ACCOMMODATE PAVING OR OTHER OPERATIONS.
29. FOR NIGHTTIME CONSTRUCTION, THE WORK SITES SHALL BE ILLUMINATED ACCORDING TO SPECIFICATION ITEM 15619.6730 M LIGHTING FOR NIGHTTIME CONSTRUCTION. THE CONTRACTOR IS REQUIRED TO SUBMIT A LIGHTING PLAN TO THE E.I.C., AT LEAST THIRTY DAYS PRIOR TO START OF NIGHTTIME OPERATIONS. UNDER THIS ITEM, THE ENTIRE AREA OF CONSTRUCTION OPERATION SHALL BE LIGHTED TO A MINIMUM OF FIFTY LUX, LEVEL I. ADDITIONAL LIGHTING IS REQUIRED TO ACHIEVE LEVEL II AND III ILLUMINANCE AS INDICATED IN THE SPECIFICATION. THE CONTRACTOR SHALL ALSO PROVIDE THE ENGINEER WITH A LIGHT METER FOR MEASURING LEVEL OF ILLUMINANCE AND CHECKING UNIFORMITY LEVELS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL TEMPORARY LIGHTING DURING THE CONSTRUCTION PERIOD. INITIALLY, ALL LIGHTS SHALL BE LEFT ON FOR 24 HOURS IN ORDER TO VERIFY THEIR PERFORMANCE.
30. VARIABLE MESSAGE DISPLAY UNITS MAY BE USED IN HIGHWAY WORK AREAS TO SUPPLEMENT REGULATORY, WARNING, OR GUIDE SIGNS. THE APPLICATION, DESIGN, OPERATION, AND LOCATION OF ANY SUCH UNITS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 294.6 OF THE NYSMUTCD.
31. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20' IN THE ACTIVE WORK SPACE.
32. UNLESS AUTHORIZED BY THE E.I.C. THE MINIMUM LANE WIDTHS FOR WORK ZONE TRAVEL LANES SHALL BE AS FOLLOWS: FREEWAYS AND/OR EXPRESSWAYS IS 11'. THE MINIMUM LANE WIDTH FOR ALL OTHER TYPES OF ROADWAYS IS 10'.

SPECIAL NOTES

1. WORK ZONE TRAFFIC CONTROL IS THE RESPONSIBILITY OF THE PERMITEE. ANY SUBCONTRACTORS WORKING FOR THE PERMITEE MUST HAVE A COPY OF THE HIGHWAY WORK PERMIT ON THE SITE AND MUST BE FAMILIAR WITH THE TRAFFIC CONTROL REQUIREMENTS. IT IS STRONGLY ADVISED THAT A "MARGATE" SAFETY MEETING WITH EACH WORK CREW BE INITIATED BEFORE THE START OF WORK.
2. WORK ZONE TRAFFIC CONTROL SCHEMES MUST BE IN PLACE AND MAINTAINED, THROUGHOUT THE DURATION OF WORK.
3. ALL WORK ZONE TRAFFIC CONTROL TO BE IN ACCORDANCE WITH THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THE NYS SUPPLEMENT.
4. ANYONE WORKING WITHIN THE HIGHWAY RIGHT-OF-WAY SHALL WEAR HIGH-VISIBILITY APPAREL MEETING THE ANSI 107-2004 CLASS II STANDARDS AND A HARD HAT.
5. AT THE START OF WORK ON THE PROJECT, ALL WORK ZONE TRAFFIC CONTROL DEVICES SHALL APPEAR IN "ACCEPTABLE" CONDITION. THESE DEVICES SHALL NOT BE ALLOWED TO FALL BELOW THE "MARGINAL" CONDITION AT ANY TIME DURING THE LIFE OF THE PROJECT. FADED AND DETERIORATED PANELS AND NON-STANDARD LEGENDS ARE NOT ACCEPTABLE.
6. NO LOW-MOUNTED SIGNS SHALL BE PERMITTED, EXCEPT FOR FLEXIBLE PANELS MEETING THE CURRENT SPECIFICATION REQUIREMENTS.
7. ACCESS TO MOUNTAINVIEW LANE AND ADJACENT COMMERCIAL/RESIDENTIAL USES MUST BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION.

LANE CLOSURE PROCEDURES AND LIMITATIONS

ALL LANE CLOSURES AND WORK ZONE TRAFFIC CONTROL MUST CONFORM TO THE NATIONAL MUTCD AND THE NYSOT MUTCD SUPPLEMENT.

LANE CLOSURE CONSTRAINTS:
LANES MAY BE CLOSED BETWEEN THE HOURS OF 9:00 AM TO 3:30 PM.

THE CONTRACTOR IS ADVISED THAT THE STATE RESERVES THE RIGHT TO PRECLUDE LANE CLOSURES DURING PERIODS OF INCLEMENT WEATHER, WET OR ICY PAVEMENT, REDUCED VISIBILITY, TRAFFIC ACCIDENTS OR ANY OTHER EMERGENCIES. THE STATE MAY ALTER ANY LANE CLOSURES SHOULD TRAFFIC CONDITIONS OR OTHER UNFORESEEN CIRCUMSTANCES ARISE WHICH WOULD ADVERSELY AFFECT THE TRAFFIC FLOW. THE CONTRACTOR IS ALSO ALERTED TO THE FACT THAT INCIDENT MANAGEMENT OR TRAFFIC CONDITIONS MIGHT FORCE HIS/HER CONSTRUCTION OPERATION TO STOP, EVEN DURING TIME WHERE SUCH OPERATION WOULD NORMALLY BE PERMITTED. THE CONTRACTOR SHALL HAVE NO CLAIM AGAINST THE STATE FOR ANY DELAYS OR EXTRA COSTS INCURRED IN COMPLYING WITH THESE RESTRICTIONS. THE STATE MAY GRANT A WAIVER OF THESE RESTRICTIONS UPON A TIMELY RECEIPT OF A REQUEST FOR WAIVER FROM THE CONTRACTOR. A MINIMUM OF FIVE WORKING DAYS FOR HTE REVIEW OF THE CONTRACTOR'S REQUEST WILL BE REQUIRED.

HOLIDAY LANE CLOSURE RESTRICTIONS:

HOLIDAY	DAY OF WEEK OF HOLIDAY	TEMPORARY LANE CLOSURES ARE NOT ALLOWED	
		FROM	TO
NEW YEAR'S DAY INDEPENDENCE DAY MEMORIAL DAY LABOR DAY	SUNDAY OR MONDAY	6:00 AM THE FRIDAY BEFORE	10:00 AM THE TUESDAY AFTER HOLIDAY
	TUESDAY	6:00 AM THE SATURDAY BEFORE	10:00 AM THE WEDNESDAY AFTER HOLIDAY
	WEDNESDAY	6:00 AM THE TUESDAY BEFORE	10:00 AM THE THURSDAY AFTER HOLIDAY
	THURSDAY	6:00 AM THE WEDNESDAY BEFORE	10:00 AM THE MONDAY AFTER HOLIDAY
	FRIDAY OR SATURDAY	6:00 AM THE THURSDAY BEFORE	10:00 AM THE MONDAY AFTER HOLIDAY
CHRISTMAS DAY	SUNDAY OR MONDAY	6:00 AM THE FRIDAY BEFORE	10:00 AM THE TUESDAY AFTER HOLIDAY
	TUESDAY	6:00 AM THE FRIDAY BEFORE	10:00 AM THE WEDNESDAY AFTER HOLIDAY
	WEDNESDAY	6:00 AM THE SATURDAY BEFORE	10:00 AM THE THURSDAY AFTER HOLIDAY
	THURSDAY	6:00 AM THE WEDNESDAY BEFORE	10:00 AM THE MONDAY AFTER HOLIDAY
THANKSGIVING DAY	FRIDAY OR SATURDAY	6:00 AM THE THURSDAY BEFORE	10:00 AM THE MONDAY AFTER HOLIDAY
	THURSDAY	6:00 AM THE WEDNESDAY BEFORE	10:00 AM THE MONDAY AFTER HOLIDAY

ADDITIONAL NOTIFICATION REQUIREMENTS:

ROAD WORK PLANS CHANGE BECAUSE OF WEATHER AND WORK SCHEDULES. THE TMC MUST BE CALLED AT THE START OF ANY CLOSURES, AND AGAIN AT THE END OF THE DAY WHEN THE LANE/SHOULDER IS REOPENED TO TRAFFIC AT (914)742-6100.

- BE PREPARED TO PROVIDE:
- PERMIT NUMBER
 - CALLER NAME
 - CALLBACK NUMBER
 - LOCATION OF ROADWORK
 - LANE BLOCKAGE DETAILS
 - TYPE OF WORK
 - PLANNED START AND FINISH TIMES FOR THE DAY

REV	DATE	COMMENTS	BY

DYNAMIC TRAFFIC, LLC
 TRAFFIC IMPACT STUDIES • ACCESS PERMITTING
 HIGHWAY & INTERSECTION DESIGN
 TRAFFIC SIGNAL & ELECTRICAL DESIGN
 245 Main Street, Suite 110
 Chester, NJ 07930
 T: 732.481.0760 | F: 908.879.0222
 www.dynamictraffic.com

13 MOUNTAIN VIEW LLC
TRAFFIC CONTROL NOTES AND DETAILS
ROADWAY IMPROVEMENT PLANS
 13 MOUNTAINVIEW AVENUE
 SECTION 74.07 - BLOCK 1 - LOTS 33 & 36
 TOWN OF ORANGETOWN, ROCKLAND COUNTY, NY

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 WWW.CALL811.COM

KEY M. SAVAGE
 PROFESSIONAL ENGINEER
 NEW YORK LICENSE NO. 093631

COREY M. CHASE
 PROFESSIONAL ENGINEER
 NEW YORK LICENSE No. 093631

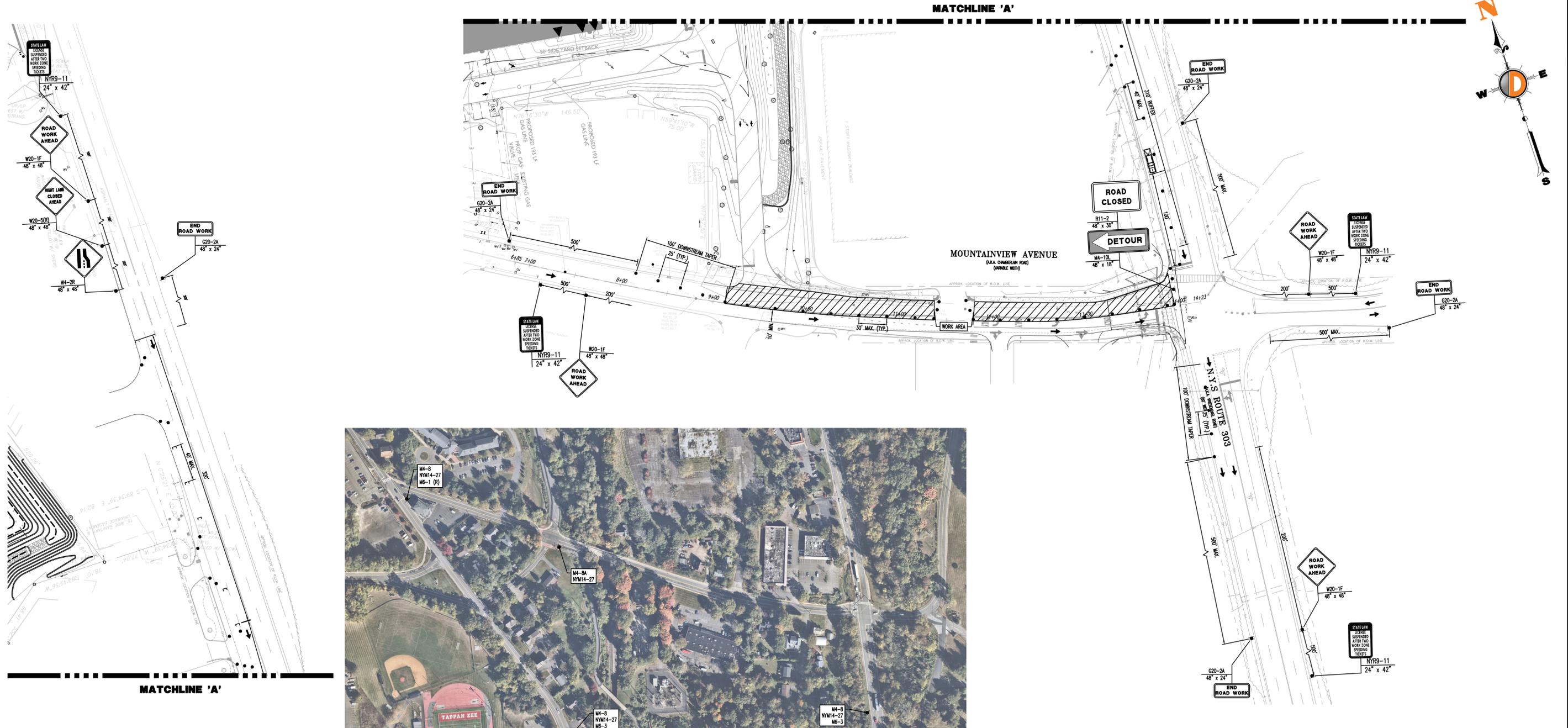
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 DESIGNED BY: ARF
 CHECKED BY: KMS

PROJECT NUMBER: 3566 24-04790
 DATE: 05/15/2025
 REV. #: 0

SHEET NUMBER:
7 OF **11**

TOP-1
TOP-3

Plotted: 05/15/25 - 10:23 AM, By: aironante, Product Ver: 25.0
 File: \\TRAFFIC PROJECTS\3566 Link Industrial Management LLC\24-04790 Orangeburg Dwg\WSDOT Traffic Control Plan\35662404790TCP0.dwg, ----> 08 TRAFFIC CONTROL PLAN - PHASE 1



REV	DATE	COMMENTS	BY

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13 MOUNTAIN VIEW LLC

TRAFFIC CONTROL PLAN - PHASE 1
ROADWAY IMPROVEMENT PLANS

13 MOUNTAINVIEW AVENUE
SECTION 74.07 - BLOCK 1 - LOTS 33 & 36
TOWN OF ORANGETOWN, ROCKLAND COUNTY, NY

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KEYNOTE M. SAWAGE

PROFESSIONAL ENGINEER
NEW YORK LICENSE No. 093631

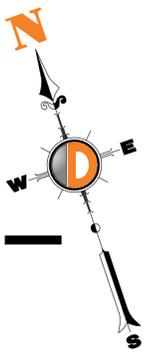
COREY M. CHASE

PROFESSIONAL ENGINEER
NEW YORK LICENSE No. 093631

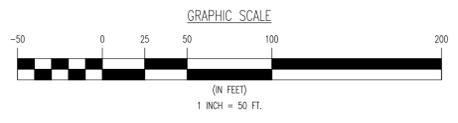
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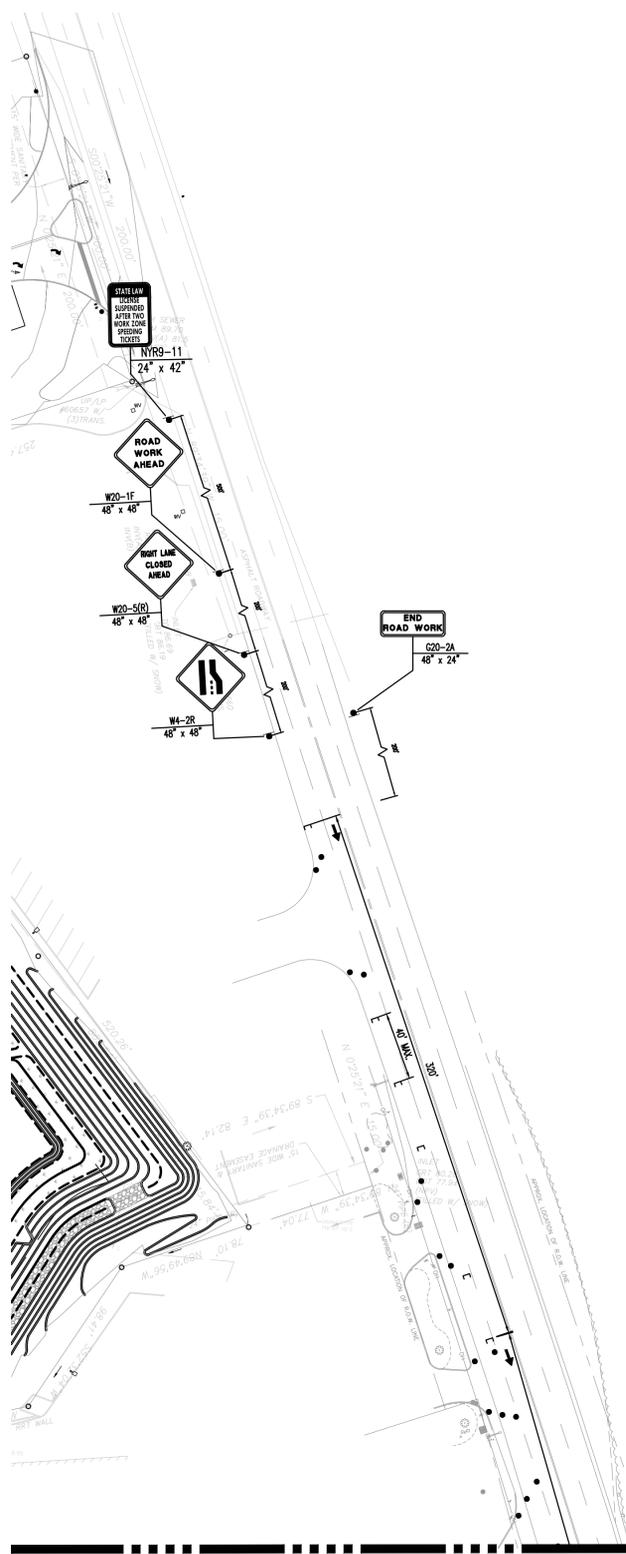
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8 OF **11**



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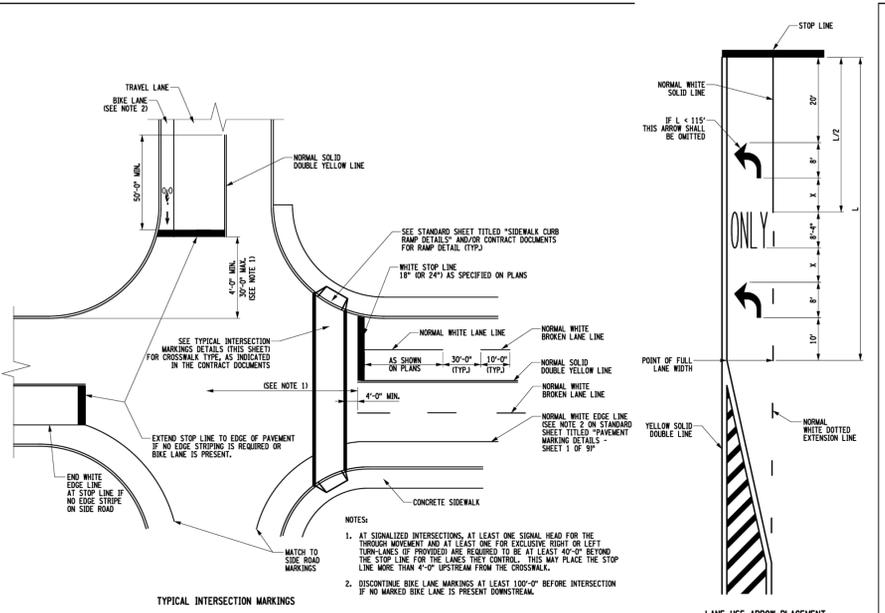
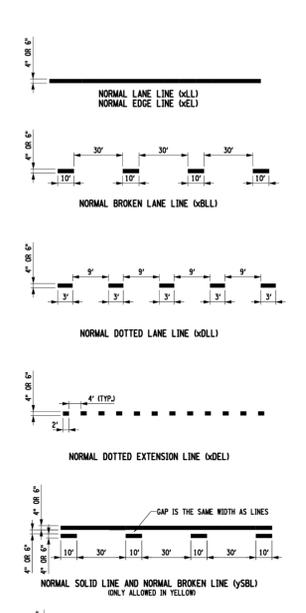
13 MOUNTAIN VIEW LLC
TRAFFIC CONTROL PLAN — PHASE 2
ROADWAY IMPROVEMENT PLANS
 13 MOUNTAINVIEW AVENUE
 SECTION 74.07 — BLOCK 1 — LOTS 33 & 36
 TOWN OF ORANGETOWN, ROCKLAND COUNTY, NY

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KEYVAN M. SAWAGE
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SCALE: 1"=50'
 PROJECT NUMBER: 3566-24-04790
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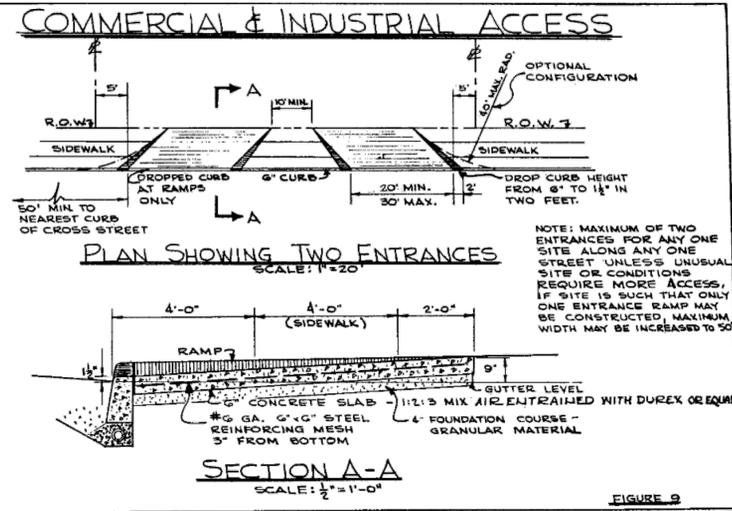
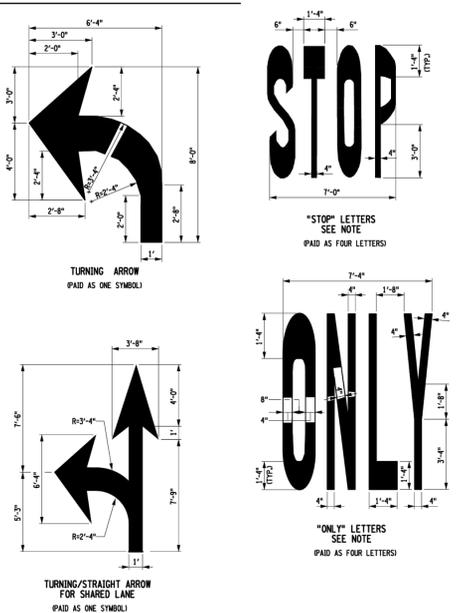
TYPICAL INTERSECTION MARKINGS

RESURFACING NOTES:

1. DETECTABLE WARNING SURFACES SHALL BE PROVIDED AT THE FOLLOWING LOCATIONS IN PAVEMENT ACCESS AREAS:
 - A. CURB RAMP AND ALIGNED TRANSITIONS AT PAVEMENT STREET CROSSINGS.
 - B. PAVEMENT ACCESS AREAS WHERE THE LENGTH OF THE PAVEMENT ACCESS ROUTE ACROSS THE RETAINING ISLAND IS GREATER THAN OR EQUAL TO 20'. PAVEMENT ACCESS AREAS INCLUDING BUT NOT LIMITED TO: LANEWAYS OR DRIVEWAYS.
 - C. PAVEMENT ACCESS AREAS WHERE THE PAVEMENT ACCESS ROUTE CROSSES DRIVEWAYS WITH SIGNAL, STOP OR STOP CONTROL.
2. DETECTABLE WARNING SURFACES SHALL BE PROVIDED WHERE THE PAVEMENT ACCESS ROUTE CROSSES DRIVEWAYS WITH SIGNAL, STOP OR STOP CONTROL.
3. CURB TYPES SHALL BE AS SHOWN ON PLANS AND SHALL BE CONFORMANT WITH THE MATERIAL AND CONSTRUCTION REQUIREMENTS OF SECTION 401 OF THE STANDARD SPECIFICATIONS.
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PAVEMENT MARKING LINE NOTES:

- 1.1. EDGE LINES ON ALL RURAL, HIGH-SPEED HIGHWAY SEGMENTS WITH POSTED SPEEDS OF 45 MPH OR MORE SHALL BE 4" WIDE. NORMAL PAVEMENT MARKING LINES SHALL BE 4" ON FREeways AND EXPRESSWAYS AND ON RAMPs TO AND FROM FREeways AND EXPRESSWAYS, AND WHERE SPECIFIED IN THE CONTRACT DOCUMENTS. AT ALL OTHER LOCATIONS NORMAL PAVEMENT MARKING LINES SHALL BE 4" WIDE. PAVEMENT MARKING LINES SHALL BE THICK AS WIDE AS NORMAL LINES ON THE SAME HIGHWAY, CHANNELING LINES ARE THE SAME WIDTH AS SIDE LINES.
- 1.2. PAVEMENT MARKING LINE CODES MAY BE SHOWN ON THE PLANS. DESIGNATIONS:
 - *W=WHITE
 - *Y=YELLOW
 - *B=BLACK



COMMERCIAL & INDUSTRIAL ACCESS

PLAN SHOWING TWO ENTRANCES
SCALE: 1"=20'

SECTION A-A
SCALE: 1/2"=1'-0"

FIGURE 2

FIGURE 3

COMMERCIAL STREET SPECIFICATIONS
SCALE: 1/2"=1'-0"

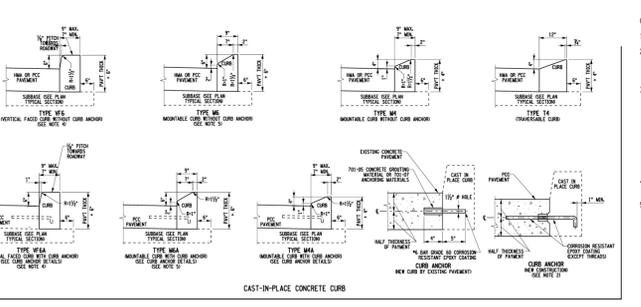
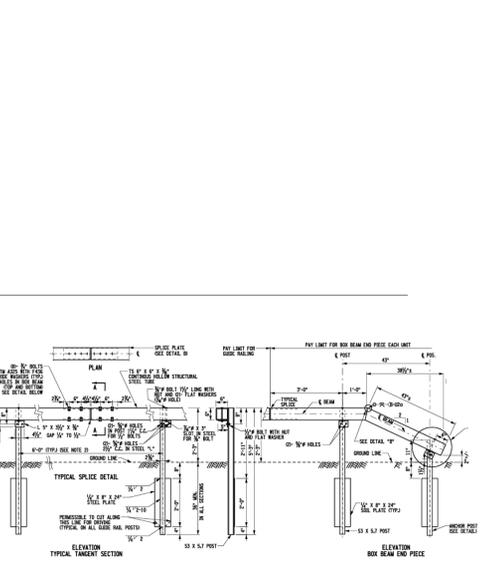
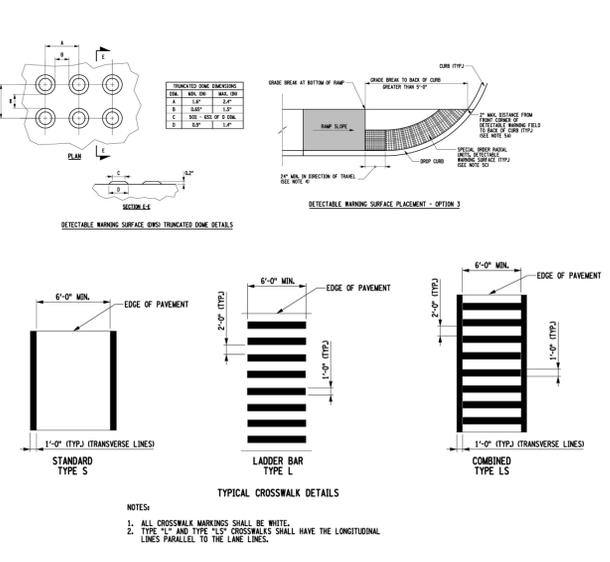
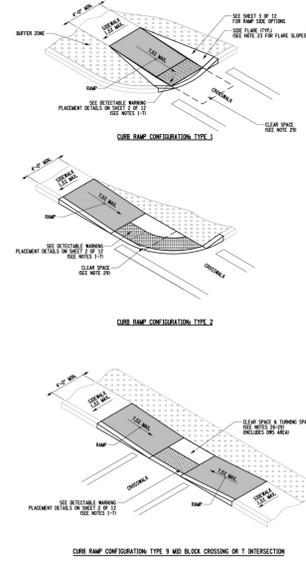
NOTE: MAXIMUM OF TWO ENTRANCES FOR ANY ONE SITE ALONG ANY ONE STREET UNLESS UNUSUAL SITE OR CONDITIONS REQUIRE MORE ACCESS. IF SITE IS SUCH THAT ONLY ONE ENTRANCE RAMP MAY BE CONSTRUCTED, MAXIMUM WIDTH MAY BE INCREASED TO 50'.

NOTE: THESE SHALL NOT BE PLANTED WITHIN ROW LINES.

SIDEWALK DETAILS

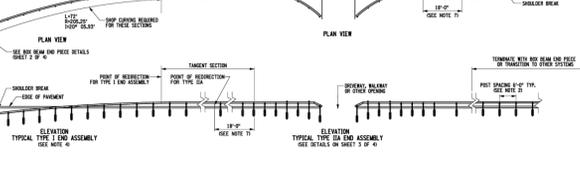
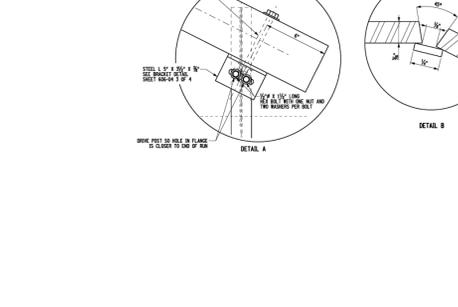
#6 GAUGE 6"x6" STEEL REINFORCING MESH SHALL BE LAID 2" FROM BOTTOM OF SIDEWALK SLAB. CONTRACTION JOINTS HAVING 1/4" RADIUS EDGES SHALL BE PLACED AT 4'-0" INTERVALS IN SIDEWALK. EXPANSION JOINTS OF 1/2" CELLULOSE OR SIMILAR MATERIAL SHALL BE PLACED AT 20' INTERVALS. EDGES SHALL HAVE 1/4" RADIUS.

NOTE: WHERE BUILDING IS NOT SET BACK OR WHERE AREA BETWEEN BUILDING AND CURB IS TO BE PAVED, CONSTRUCT 10 FT. SIDEWALK, EITHER THAN 4' AS SHOWN ON SKETCH.



GENERAL PAVEMENT MARKING NOTES:

1. ALL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH THE METHOD AND WYS SPECIFICATION.
2. EDGE LINES SHALL BE YELLOW ON THE LEFT SIDE AND WHITE ON THE RIGHT SIDE IN THE DIRECTION OF TRAVEL UNLESS OTHERWISE SHOWN ON PLANS. IF THE CURB OFFSET IS LESS THAN 2'-0" THE EDGE LINE SHALL BE APPLIED ADJACENT TO CURB UNLESS OTHERWISE SHOWN ON PLANS. EDGE LINES SHALL BE PROVIDED AT THE CURB ADJACENT TO RAISED ISLANDS SEE DETAIL 14.
3. WHERE MARKINGS NORMALLY FOLLOW A PAVEMENT JOINT, SINGLE LINE MARKINGS SHALL BE PLACED ALONG THE SIDE OF THE JOINT. DOUBLE LINE MARKINGS SHALL STRADDLE THE JOINT. LINE LINES ON ROADWAYS WHICH ARE MORE THAN TWO LANES WIDE AND HAVE LONGITUDINAL JOINTS BETWEEN ADJACENT LANES SHALL BE PLACED ON THE SIDE OF THE JOINT WHICH WILL OBTAIN OPTIMUM LANE WIDTHS.
4. AT THE JUNCTION OF SINGLE AND DOUBLE LINE MARKINGS WHICH FOLLOW A PAVEMENT JOINT, THE SINGLE LINE SHALL BE AN EXTENSION OF EITHER OF THE DOUBLE LINES AND NOT THE SPACE BETWEEN THEM. AT THE JUNCTION OF SINGLE AND DOUBLE LINE MARKINGS WHICH DO NOT FOLLOW A PAVEMENT JOINT, THE SINGLE LINE SHALL BE ALIGNED WITH THE CENTER OF THE DOUBLE LINE MARKING OR WITH EITHER LINE OF THE DOUBLE LINE.
5. ALL DIMENSIONS AND THE PLACEMENT OF MARKINGS, SYMBOLS, AND TEXT SHOWN ARE TYPICAL AND SHALL APPLY UNLESS OTHERWISE INDICATED IN THE CONTRACT DOCUMENTS.
6. THE REGIONAL TRAFFIC ENGINEER WILL REVIEW AND APPROVE ANY CHANGES TO THE PAVEMENT MARKING PLANS PRIOR TO FINAL INSTALLATION. CHANGES SHALL BE SUBMITTED TWO WEEKS PRIOR TO INSTALLATION.



REV	DATE	COMMENTS	BY

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13 MOUNTAIN VIEW LLC

CONSTRUCTION DETAILS
ROADWAY IMPROVEMENT PLANS

13 MOUNTAINVIEW AVENUE
SECTION 74.07 - BLOCK 1 - LOTS 33 & 36
TOWN OF ORANGETOWN, ROCKLAND COUNTY, NY

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FOR STATE SPECIFIC DIRECT PHONE NUMBERS VISIT: WWW.CALL811.COM

KEYNES M. SAWAGE

PROFESSIONAL ENGINEER
NEW YORK LICENSE NO. 093

COREY M. CHASE

PROFESSIONAL ENGINEER
NEW YORK LICENSE NO. 093631

SCALE: AS SHOWN
DRAWN BY: C.M.
DESIGNED BY: A.R.F.
CHECKED BY: K.M.S.

PROJECT NUMBER: 3586 24-04790
DATE: 05/15/2025
REV: 0

SHEET NUMBER: **10** OF **11**

DEFINITION OF TERMS
BARRIER - A device used to prevent traffic from crossing a roadway boundary or to prevent traffic from crossing a boundary between two roadways.

CONTRACTOR'S RESPONSIBILITIES
1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DESIGN AND CONSTRUCTION OF THE ROADWAY IMPROVEMENTS AND FOR THE PROTECTION OF THE EXISTING ROADWAY AND UTILITIES.

GENERAL NOTES ON STANDARD SHEETS
1. THE STANDARD SHEETS SHALL BE USED IN CONJUNCTION WITH THE STANDARD SPECIFICATIONS FOR ROADWAY CONSTRUCTION AND THE STANDARD SPECIFICATIONS FOR ROADWAY MAINTENANCE.

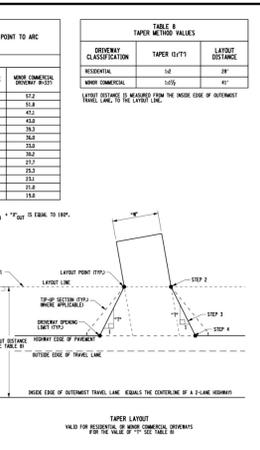
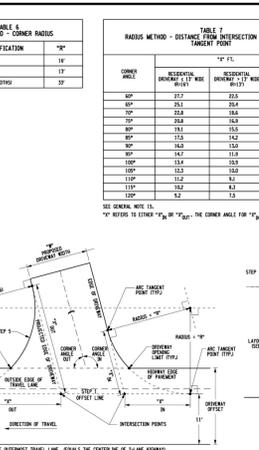
TABLE 1 RECOMMENDED DRIVEWAY WIDTH "W"
DRIVEWAY CLASSIFICATION, RECOMMENDED DRIVEWAY WIDTH "W", MINIMUM DRIVEWAY WIDTH "W"

TABLE 2 MAXIMUM DRIVEWAY SLOPE
SLOPE CLASSIFICATION, MAXIMUM DRIVEWAY SLOPE

TABLE 3 DRIVEWAY MATERIALS AND THICKNESS
PROPOSED EXISTING DRIVEWAY, WITHIN DRIVEWAY DRIVEWAY LENGTH "L", WITHIN TRAVEL LANE LENGTH "L"

TABLE 4 DRIVEWAY ENTRANCE TYPE SELECTION
DRIVEWAY TYPE, ENTRANCE WIDTH, DRIVEWAY MATERIALS AND THICKNESS

TABLE 5 MINIMUM LENGTH OF RETAINING CURB, "L"
CHANGES IN GRADE, DRIVEWAY PROFILE



DESIGN ELEMENT TOLERANCES
ELEMENT, TOLERANCE



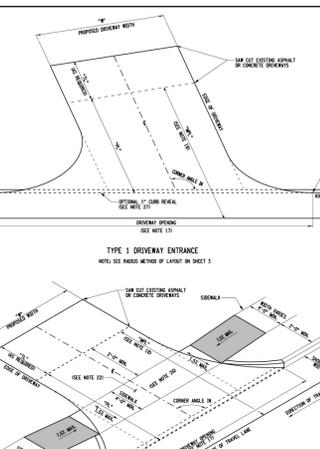
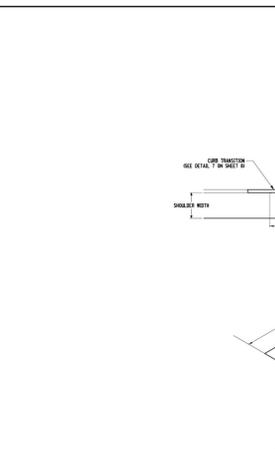
NEW YORK STATE DEPARTMENT OF TRANSPORTATION
RESIDENTIAL AND MINOR COMMERCIAL DRIVEWAYS
SHEET 1 OF 3



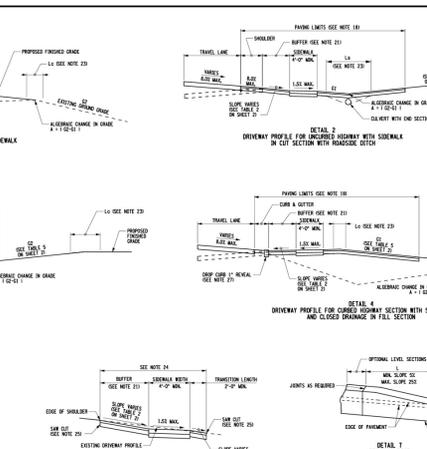
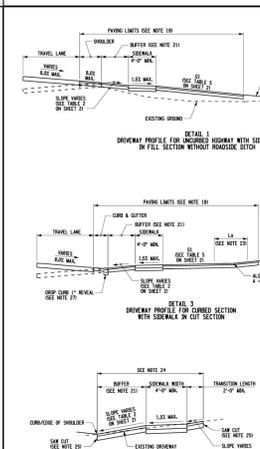
NEW YORK STATE DEPARTMENT OF TRANSPORTATION
RESIDENTIAL AND MINOR COMMERCIAL DRIVEWAYS
SHEET 2 OF 3



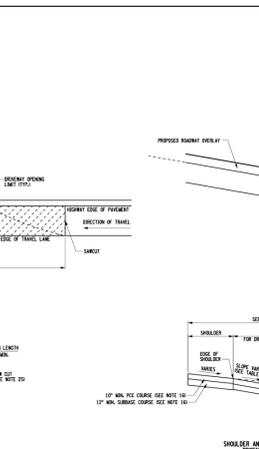
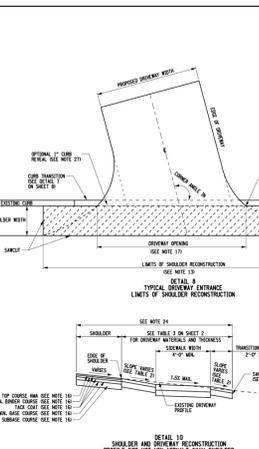
NEW YORK STATE DEPARTMENT OF TRANSPORTATION
RESIDENTIAL AND MINOR COMMERCIAL DRIVEWAYS
SHEET 3 OF 3



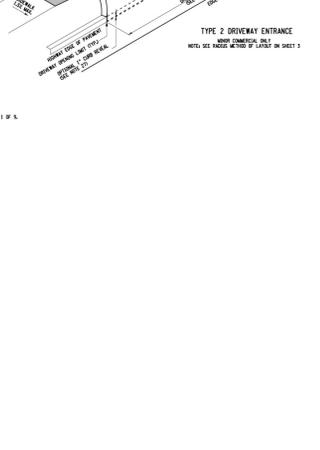
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SHEET 5 OF 5



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SHEET 8 OF 9



NEW YORK STATE DEPARTMENT OF TRANSPORTATION
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NEW YORK STATE DEPARTMENT OF TRANSPORTATION
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SHEET 3 OF 3



NEW YORK STATE DEPARTMENT OF TRANSPORTATION
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SHEET 8 OF 9



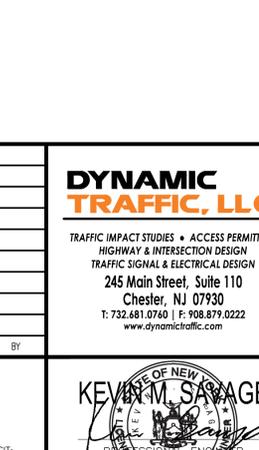
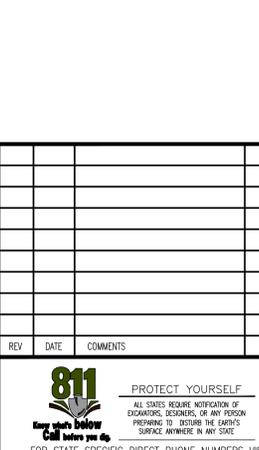
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SHEET 9 OF 9



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SHEET 5 OF 5



NEW YORK STATE DEPARTMENT OF TRANSPORTATION
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SHEET 8 OF 9



NEW YORK STATE DEPARTMENT OF TRANSPORTATION
RESIDENTIAL AND MINOR COMMERCIAL DRIVEWAYS
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SHEET 9 OF 9

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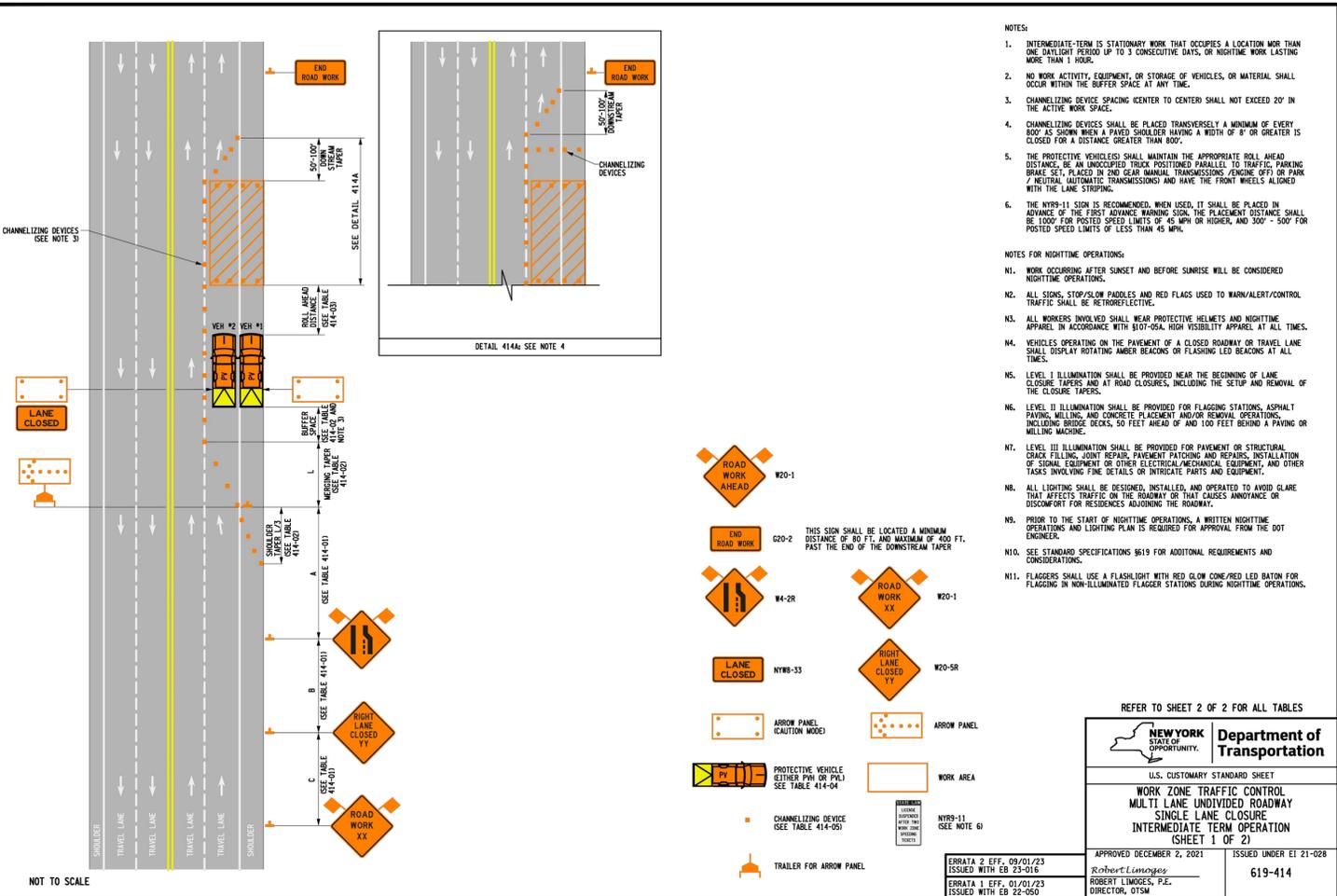
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Plotted: 05/16/25 - 1:42 PM, By: achen, Product Ver: 25.0a (LMS Tech)
 File: T:\TRAFFIC PROJECTS\3566 Link Industrial Management LLC\24-04790 Orangeburg Dwg\WSDOT Traffic Control Plan\Collin's Site Plan TCP\35662404790TCP1.dwg, ---> T2 TRAFFIC CONTROL PLAN



NOTES:

- INTERMEDIATE-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.
- NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
- CHANNELIZING DEVICE SPACING CENTER TO CENTER SHALL NOT EXCEED 20' IN THE ACTIVE WORK SPACE.
- CHANNELIZING DEVICES SHALL BE PLACED TRANSVERSELY TO TRAFFIC. THE SPACING BETWEEN DEVICES SHALL BE A MINIMUM OF EVERY 8' OR GREATER AS SHOWN WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 400'.
- THE PROTECTIVE VEHICLES SHALL MAINTAIN THE APPROPRIATE ROLL AHEAD DISTANCE, BE AN UNOCCUPIED TRUCK POSITIONED PARALLEL TO TRAFFIC, PARKING BRAKE SET, PLACED IN 2ND GEAR MANUAL TRANSMISSIONS, ENGINE OFF OR PARK / NEUTRAL, AUTOMATIC TRANSMISSIONS AND HAVE THE FRONT WHEELS ALIGNED WITH THE LANE STRIPING.
- THE NYR9-11 SIGN IS RECOMMENDED. WHEN USED, IT SHALL BE PLACED IN ADVANCE OF THE FIRST ADVANCE WARNING SIGN. THE PLACEMENT DISTANCE SHALL BE 100' FOR POSTED SPEED LIMITS OF 45 MPH OR HIGHER, AND 300' - 500' FOR POSTED SPEED LIMITS OF LESS THAN 45 MPH.

NOTES FOR NIGHTTIME OPERATIONS:

- WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.
- ALL SIGNS, STOP/SLOW PADDOLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.
- ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMETS AND NIGHTTIME APPAREL IN ACCORDANCE WITH 910T-05A, HIGH VISIBILITY APPAREL AT ALL TIMES.
- VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING RED BEACONS AT ALL TIMES.
- LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.
- LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.
- LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL EQUIPMENT AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.
- ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENTS ADJOINING THE ROADWAY.
- PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL FROM THE DOT ENGINEER.
- SEE STANDARD SPECIFICATIONS 9619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS.
- FLAGGERS SHALL USE A FLASHLIGHT WITH RED CLOW CONE/RED LED BAYON FOR FLAGGING IN NON-ILLUMINATED FLAGGER STATIONS DURING NIGHTTIME OPERATIONS.
- SEE STANDARD SPECIFICATIONS 9619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS.

REFER TO SHEET 2 OF 2 FOR ALL TABLES

NEW YORK STATE DEPARTMENT OF TRANSPORTATION

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL

MULTI LANE UNDIVIDED ROADWAY

SINGLE LANE CLOSURE

INTERMEDIATE TERM OPERATION

(SHEET 1 OF 2)

APPROVED DECEMBER 2, 2021 ISSUED UNDER EIT 21-028

ERRATA 2 EFF. 09/01/23 ISSUED WITH EB 23-016 Robert Limoges, P.E. DIRECTOR, OTSM

ERRATA 1 EFF. 01/01/23 ISSUED WITH EB 22-050 619-414

TABLE 414-01: ADVANCE WARNING SIGN SPACING

ROAD TYPE	DISTANCE BETWEEN SIGNS			SIGN LEGEND	
	A @TJ	B @TJ	C @TJ	XX	YY
URBAN (≤ 30 MPH)	100	100	100	AHEAD	AHEAD
URBAN (35-40 MPH)	200	200	200	AHEAD	AHEAD
URBAN (45 MPH)	350	350	350	1000 FT. AHEAD	1000 FT. AHEAD
RURAL	500	500	500	1500 FT. AHEAD	1000 FT. AHEAD

* PRECONSTRUCTION POSTED SPEED LIMIT

TABLE 414-02: LONGITUDINAL BUFFER SPACE AND TAPER LENGTHS

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	LONGITUDINAL BUFFER SPACE (FT.)	TAPER LENGTH (FT.) * OF SKIP LINES / DISTANCE (FT.) / LATERAL SHIFT OF TRAFFIC FLOW PATH			SHOULDER TAPER LENGTH (FT.) * OF SKIP LINES / * OF CHANNELIZING DEVICES						
		FOR LANE WIDTH IN FT.	FOR SHOULDER WIDTH		FOR SHOULDER WIDTH						
		10	11	12	≤ 4 FT.	5 - 7 FT.	8 FT.	9 FT.	10 FT.	11 FT.	12 FT.
25	155/4	120/3/4	120/3/4	120/3/4	40/1/2	40/1/2	40/1/2	40/1/2	40/1/2	40/1/2	40/1/2
30	200/5	160/4/5	160/4/5	160/4/5	40/1/2	40/1/2	40/1/2	40/1/2	40/1/2	40/1/2	40/1/2
35	250/6	200/5/6	240/6/7	240/6/7	40/1/2	40/1/2	80/2/3	80/2/3	80/2/3	80/2/3	80/2/3
40	305/8	280/7/8	320/8/9	320/8/9	40/1/2	40/1/2	80/2/3	80/2/3	80/2/3	80/2/3	120/3/4
45	360/9	440/11/12	520/13/14	560/14/15	80/2/3	80/2/3	80/2/3	120/3/4	120/3/4	120/3/4	160/4/5
50	425/11	520/13/14	560/14/15	600/15/16	80/2/3	120/3/4	160/4/5	160/4/5	160/4/5	160/4/5	160/4/5
55	495/13	560/14/15	600/15/16	680/17/18	80/2/3	120/3/4	160/4/5	160/4/5	160/4/5	200/5/6	200/5/6

TABLE 414-03: ROLL AHEAD DISTANCE FOR PROTECTIVE VEHICLES

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	STATIONARY OPERATION	
	PROTECTIVE VEHICLES WEIGHING 9500 TO 21,999 LBS. GVW	PROTECTIVE VEHICLES WEIGHING 22,000 LBS. OR GREATER GVW
45 - 55	160/4	120/3
≤ 40	120/3	80/2

TABLE 414-04: PROTECTIVE VEHICLE REQUIREMENTS

CLOSURE TYPE	ROAD TYPE & SPEED	NON-FREEMAN	
		≥ 45 MPH	35 - 40 MPH ≤ 30 MPH
LANE CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR WORK VEHICLE EXPOSED TO TRAFFIC	PWH+TMA	PVL+TMA SEE NOTE 2
	-NO WORKERS ON FOOT -NO WORK VEHICLE EXPOSED TO TRAFFIC -OTHER HAZARDS EXPOSED (IE EQUIPMENT, MATERIALS)	PWH+TMA SEE NOTE 2	SEE NOTE 2
SHOULDER CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR WORK VEHICLE EXPOSED TO TRAFFIC	PWH+TMA	SEE NOTE 2
	-NO WORKERS ON FOOT -NO WORK VEHICLE EXPOSED TO TRAFFIC -OTHER HAZARDS EXPOSED (IE EQUIPMENT, MATERIALS, EXCAVATION)	SEE NOTE 3	SEE NOTE 2

LEGEND:

- PVL - PROTECTIVE VEHICLE LIGHT MINIMUM GROSS WEIGHT 9500 LBS. OR GREATER (SEE NOTE 5)
- PWH - PROTECTIVE VEHICLE HEAVY MINIMUM GROSS WEIGHT 22,000 LBS. OR GREATER
- TMA - TRUCK/TRAILER MOUNTED IMPACT ATTENUATOR

NOTES:

- THE EXPOSURE CONDITIONS ASSUME THERE IS NO POSITIVE PROTECTION PRESENT.
- EITHER A PROTECTIVE LIGHT (PVL) OR THE STANDARD BUFFER SPACE (SEE TABLE 011-03) SHALL BE PROVIDED.
- EITHER A PROTECTIVE LIGHT (PVL) OR THE STANDARD BUFFER SPACE (SEE TABLE 011-03) SHALL BE PROVIDED.
- TRUCK/TRAILER MOUNTED IMPACT ATTENUATORS (TMA) SHALL NOT BE MOUNTED/INSTALLED ON VEHICLES WITH A GROSS VEHICLE WEIGHT (GVW) LESS THAN WHAT IS MINIMALLY REQUIRED BY THE MANUFACTURER OF THE TMA.
- THE USE OF A PROTECTIVE VEHICLE LIGHT (PVL) AS A SHADOW VEHICLE IS LIMITED TO NON-FREEMAN ROADWAYS WHERE THE POSTED SPEED LIMITS IS ≤ 40 MPH UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.

TABLE 414-05: CHANNELIZING DEVICE APPLICATION FOR INTERMEDIATE-TERM STATIONARY WORK ZONES

WORK ZONE PROVISIONS	MAXIMUM DEVICE SPACING CENTER TO CENTER	MUTOC COMPLIANT CHANNELIZING DEVICES								
		DRUMS	STANDARD CONES	TALL CONES	EXTRA TALL CONES	TRUCK/MOUNTED TUBULAR MARKERS	INTERMEDIATE TUBULAR MARKERS	VERTICAL PANELS	OVERSIZED VERTICAL PANELS	TYPE III BARRICADES
INTERMEDIATE-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR	20 FT. *	X							X	
SHOULDER/ENCROACHMENT/SHIFTING TAPERS	40 FT.	X							X	
MARKING FOR TRANSVERSE BUMPS ¹	N/A	X ²			X ²				X ²	
TRANSVERSE DEVICE WITHIN CLOSED TRAFFIC LANE AND/OR SHOULDER	800 FT.	X	X	X	X			X	X	0
REMOVAL OF EXISTING GUIDE RAIL	80 FT. / 40 FT.	X	X	X	X			X	X	0

NOTES:

- * ALLOWED, BLANK = NOT ALLOWED, 0 = OPTIONAL
- 1 - A TYPE I OBJECT MARKER MAY BE USED IN LIEU OF CHANNELIZING DEVICE.
- 2 - CHANNELIZING DEVICES SHALL BE EQUIPPED WITH A FLASHING WARNING LIGHT.

TABLE 414-06: REQUIRED SIGN SIZES*

SIGN	NON-FREEMAN	FREEMAN
G20-2	36x18	48x24
NYR9-11	24x12	48x24
W20-5R	48x24	48x24
W4-2R	36x36	48x48
W20-1	36x36	48x48
W20-5R	36x36	48x48
WARNING FLAG	18x18	18x18

* FREEMAN SIZES MAY BE USED ON NON-FREEMAN, IF SPACE CONSTRAINTS DO NOT EXIST.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL

MULTI LANE UNDIVIDED ROADWAY

SINGLE LANE CLOSURE

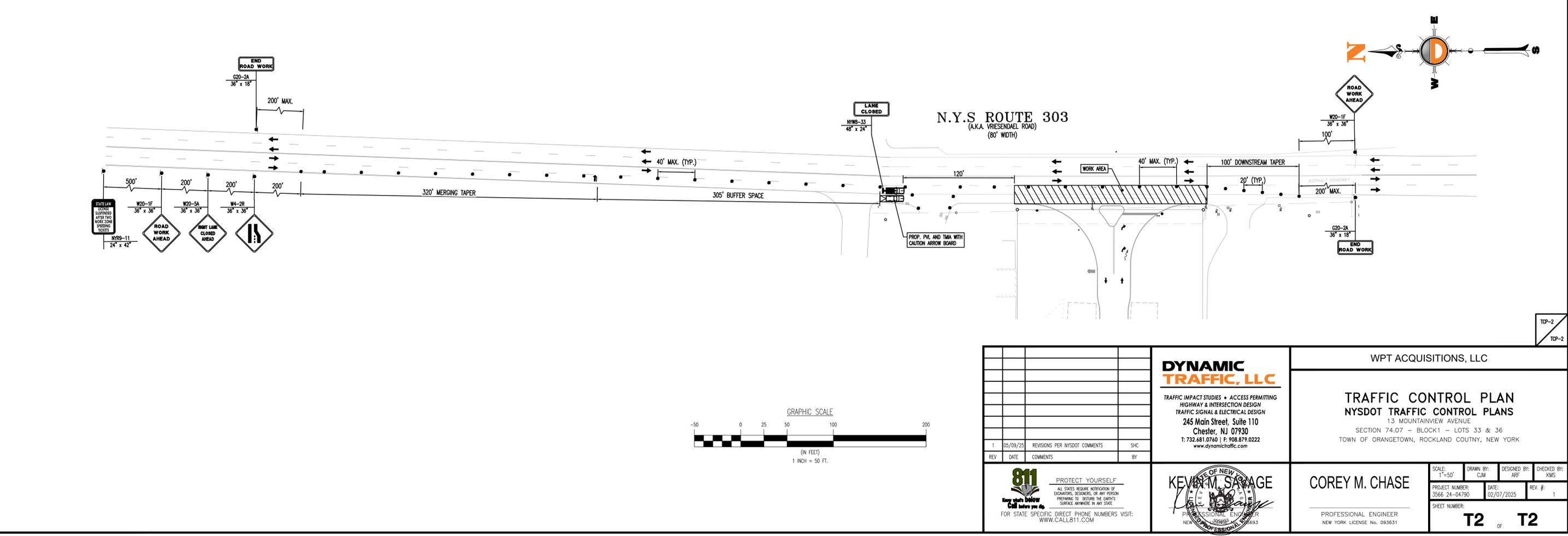
INTERMEDIATE TERM OPERATION

(SHEET 2 OF 2)

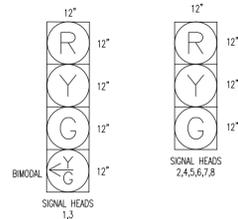
APPROVED APRIL 8, 2022 ISSUED UNDER EIT 22-008

ERRATA 1 EFF. 09/01/23 ISSUED WITH EB 23-016 Robert Limoges, P.E. DIRECTOR, OTSM

619-414



SIGNAL LEGEND



PHASE SEQUENCE

PHASE	FACE								PED. A	PED. B	
	1	2	3	4	5	6	7	8			
#1	G/C-	G	R	R	R	R	R	R	R	*	DW
#2	R	R	G/C-	G	R	R	R	R	R	R	DW
#3	R	R	R	R	G	G	G	G	G	R	DW
FLASHING OPERATION	Y	Y	Y	Y	R	R	R	R	R	R	DARK

* THIS INDICATION SHALL BE A "WALK" SYMBOL FOLLOWED BY A FLASHING "DON'T WALK" SYMBOL AND A STEADY "DON'T WALK" SYMBOL, WHEN AN ASSOCIATED PEDESTRIAN BUTTON IS ACTIVATED.

SIGNAL NOTES

- THE CONTRACTOR SHALL PERFORM ALL WORK FROM WITHIN THE STATE HIGHWAY BOUNDARY.
- THE CONTRACTOR SHALL INSTALL PUSH BUTTON EXTENSION ARM TO MAKE THE BUTTON REACHABLE. COST TO BE INCLUDED IN THE BID PRICE FOR ITEM 680.8151, ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITHOUT POST.

TABLE OF DETECTORS

SIGNAL NO.	SOLC CHANNEL	FUNCTION	DETECTION ZONE NO.	SIZE	REMARKS
42	#2		42	19.5' x 40'	PRESENCE
43	#3		43	21.5' x 40'	PRESENCE
49	#1		49	4' x 6'	SB L COUNT
50	#1		50	4' x 6'	SB T COUNT
53	#3		53	4' x 6'	EB L COUNT
54	#3		54	4' x 6'	EB T COUNT
55	#3		55	5' x 6'	EB T COUNT
56	#3		56	6' x 6'	EB T COUNT
57	#2		57	4' x 6'	NB L COUNT
58	#2		58	4' x 6'	NB T COUNT
59	#2		59	12' x 6'	NB T COUNT
61	#3		61	5.5' x 6'	WB L COUNT
62	#3		62	5.5' x 6'	WB T COUNT

TABLE OF PULLBOXES

PULLBOX NO.	680.510501	680.5100010	680.78010005	680.82250801	NOTES
PB2	RECTANGULAR 26X18 RC	CLEAN EXISTING PULLBOX	ALTER PULLBOX FOR CONDUITS	REMOVE TRAFFIC SIGNAL PULLBOXES	EXISTING
PB3	-	-	-	-	EXISTING
PB4	1	-	-	-	PROPOSED

TABLE OF CONDUITS

CONDUIT NO.	LOCATION		560.11080319	680.520106	680.520106	680.53010010	NOTES
	FROM	TO	ABANDONED ANCHOR/CONDUIT REMOVAL AND PATCHING	2" NPS STEEL	3" NPS STEEL	CLEAN EXISTING CONDUIT	
C1	S1	PB1	-	-	-	6	EXISTING
C2	PB1	PB2	-	-	-	72	EXISTING
C3	P1	PB2	-	-	-	-	EXISTING
C4	PB2	PB3	57	-	-	-	EXISTING
C5	P2	PB3	2	-	-	-	EXISTING
C6	PB2	PB4	-	-	74	-	PROPOSED
C7	P3	PB4	-	3	-	-	PROPOSED

TABLE OF CABLES

ROUTE	FROM	THROUGH	TO	CABLE DESCRIPTION	# OF CABLES	680.730214	680.730514
						2 C, 14 AWG	5 C, 14 AWG
CONTROLLER	C-1, PB-1, C-2, PB-2, C-6, PB-4, C-7		PED P-3	2 C, 14 AWG 5 C, 14 AWG	1 1	154	154

ESTIMATE OF QUANTITIES

ITEM	DESCRIPTION	UNIT	QUANTITY
206.03	CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION	LF	76
608.21000003	CAST IRON EMBEDDED DETECTABLE WARNING UNITS	SY	1.1
645.61	OVERHEAD SIGN PANELS	SF	7.5
680.5001	POLE EXCAVATION AND CONCRETE FOUNDATION	CY	1.1
680.510501	PULLBOX - RECTANGULAR, 26 X 18 INCH, REINFORCED CONCRETE	EA	1
680.51100010	CLEAN EXISTING PULLBOX	EA	1
680.520106	CONDUIT, METAL STEEL, ZINC COATED, 2"	LF	3
680.520108	CONDUIT, METAL STEEL, ZINC COATED, 3"	LF	74
680.53010010	CLEAN EXISTING CONDUIT	LF	78
680.6808	TRAFFIC SIGNAL POLE - BRACKET MOUNT, 8 FEET MOUNTING HEIGHT	EA	1
680.730214	SIGNAL CABLE, 2 CONDUCTORS, 14 AWG	LF	154
680.730514	SIGNAL CABLE, 5 CONDUCTORS, 14 AWG	LF	154
680.78010005	ALTER PULLBOX FOR CONDUITS	EA	1
680.813103	PEDESTRIAN SIGNAL SECTION - TYPE L, 12 INCH	EA	1
680.8141	PEDESTRIAN SIGNAL BRACKET MOUNT ASSEMBLY	EA	1
680.815002	PEDESTRIAN SIGNAL MODULE - 16-INCH BY 18-INCH, BI-MODAL HAND/MAN SYMBOLS WITH COUNTDOWN	EA	1
680.8151	ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITHOUT POST	EA	1
680.8223	BREAKAWAY TRANSFORMER BASE	EA	1
680.82250308	REMOVE AND DISPOSE PEDESTRIAN PUSH BUTTONS AND SIGNS	EA	1
680.82250608	REMOVE AND DISPOSE PEDESTRIAN POLE AND FOUNDATION	EA	1

TABLE OF PEDESTRIAN SIGNS

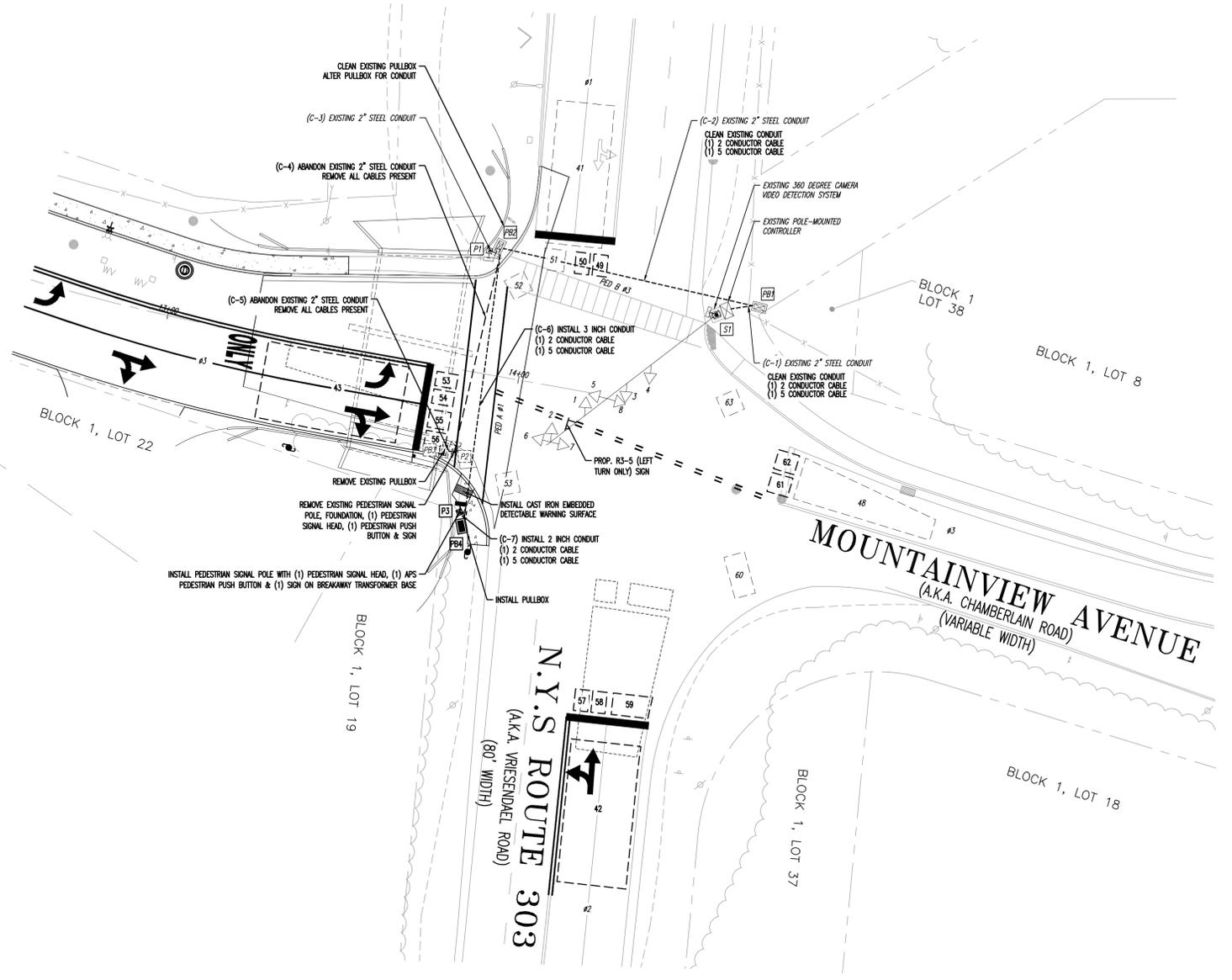
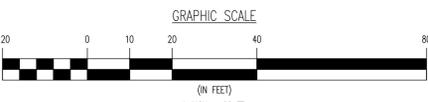
LOC.	ITEM	MOUNT	TEXT
P-3	680.8151	R10-3R	9' x 15'

TABLE OF PEDESTRIAN INDICATION ITEMS

ITEM NUMBER	DESCRIPTION	UNIT	PED-3
680.5001	POLE EXC AND CONCRETE FOUNDATION (CODE J1)	CY	1.1
680.6808	TRAFFIC SIGNAL POLE, BRKT MNT, 8 FEET MNT HEIGHT	EA	1
680.813103	PED SIGNAL SECTION - TYPE L, 12 INCH	EA	2
680.8141	PED SIGNAL BRKT MOUNT ASSY	EA	1
680.815002	PEDESTRIAN SIGNAL MODULE, 16-INCH BY 18-INCH, BI-MODAL HAND/MAN SYMBOLS WITH COUNTDOWN	EA	1
680.8151	ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITHOUT POST	EA	1
680.8223	BREAKAWAY TRANSFORMER BASE	EA	1

SIGNAL EQUIPMENT LEGEND

- PROPOSED TRAFFIC SIGNAL HEAD
- PROPOSED TRAFFIC SIGNAL HEAD WITH LOUVERS
- PROPOSED PEDESTRIAN SIGNAL HEAD
- PROPOSED PUSH BUTTON
- PROPOSED IMAGE DETECTOR
- PROPOSED RADAR DETECTOR
- PROPOSED MAST ARM SIGN
- PROPOSED POLE MOUNTED LUMINAIRE
- PROPOSED CONTROLLER CABINET
- PROPOSED PULLBOX
- PROPOSED CAST ALUMINUM JUNCTION BOX
- PROPOSED PEDESTRIAN POLE FOUNDATION
- PROPOSED STRAIN POLE FOUNDATION
- EXISTING TRAFFIC SIGNAL HEAD
- EXISTING TRAFFIC SIGNAL HEAD WITH LOUVERS
- EXISTING PEDESTRIAN SIGNAL HEAD
- EXISTING PUSH BUTTON
- EXISTING IMAGE DETECTOR
- EXISTING RADAR DETECTOR
- EXISTING MAST ARM SIGN
- EXISTING POLE MOUNTED LUMINAIRE
- EXISTING CONTROLLER CABINET
- EXISTING PULLBOX
- EXISTING CAST ALUMINUM JUNCTION BOX
- EXISTING PEDESTRIAN POLE FOUNDATION
- EXISTING STRAIN POLE FOUNDATION



Plotted: 05/06/25 - 2:44 PM, By: afevents, Product Ver: 25.0a (LMS Tech)
 File: \\TRAFFIC PROJECTS\3566 Link Industrial Management LLC\24-04790 Orangeburg Dwg\WSDOT Signal Plans\35662404790ISP1.dwg, ---> 01 TRAFFIC SIGNAL MODIFICATION PLAN (R-86)

<p>DYNAMIC TRAFFIC, LLC</p> <p>TRAFFIC IMPACT STUDIES • ACCESS PERMITTING HIGHWAY & INTERSECTION DESIGN TRAFFIC SIGNAL & ELECTRICAL DESIGN</p> <p>245 Main Street, Suite 110 Chester, NJ 07930 T: 732.681.0760 F: 908.879.0222 www.dynamictraffic.com</p>	<p>13 MOUNTAIN VIEW LLC</p> <p>TRAFFIC SIGNAL MODIFICATION PLAN (R-86) PROPOSED SIGNAL MODIFICATIONS NYS ROUTE 303 AND MOUNTAINVIEW AVENUE TOWN OF ORANGETOWN, ROCKLAND COUNTY, NY</p>	
	<p>SCALE: 1"=20'</p> <p>PROJECT NUMBER: 3566 24-04790</p> <p>SHEET NUMBER: 1 OF 1</p>	<p>DESIGNED BY: ARF</p> <p>CHECKED BY: KMS</p> <p>DATE: 01/24/2025</p> <p>REV. #: 1</p>