



Pearl River TOD District Design Guidelines

Town of Orangetown, New York

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INTRODUCTION

The Pearl River Transit Oriented Development District (“TOD”) District provides a unique opportunity to support new mixed-use development focused around the Pearl River train station. It is an opportunity to upgrade and revitalize this older historic commercial area to serve as a catalyst for orderly redevelopment, while preserving the District’s unique character. This area has been identified in the *2003 Orangetown Comprehensive Plan* as a “mixed use downtown area, by adding additional opportunities for development, while limiting and controlling retail uses outside its immediate confines.”¹

The *2018 Pearl River TOD Opportunities Study* acknowledges that the “*Pearl River is well positioned physically and economically for some kind of infill redevelopment, especially in and around the station area. However, the scale must be in keeping with the community character.*”²

The 2019 Pearl River TOD District regulations establish 6 TOD Districts that build on these prior studies. The TOD-MUR₁, TOD-MUR₂, TOD-MUR₃ Districts include a mix of retail, office, and service uses with residential development, primarily focused on existing commercial areas along Central Avenue, Main Street, and the train station. Bracketing the mixed use districts is the TOD-OR₁, TOD-OR₂, TOD-OR₃ Districts which focus on office uses combined with residential uses. The residential density progressively increases closest to the train station. Also, the TOD-MUR₃ and TOD-OR₃ Districts encompass larger properties that include older office and industrial uses. These properties require special design considerations.

These Design Guidelines support the overall planning goals of the TOD District as follows:

- To use the Pearl River train station as a catalyst for future redevelopment in the heart of the Pearl River Hamlet.
- To promote redevelopment around the Pearl River train station to maintain a diversity of entertainment, retail and service uses together with additional housing opportunities.
- To guide future development in accordance with a plan of mixed compatible and complementary land uses and appropriate development standards in keeping with the character and scale of the Pearl River Hamlet.
- To support more diverse housing choices in the downtown area near the train station.
- To adopt design standards to maintain and enhance the architectural character of the Pearl River TOD District, so that downtown Pearl River may realize its potential as an attractive place to live and to work.
- To protect and conserve the value of land and encourage revitalization of the buildings in the Pearl River TOD District.

The historic character of the TOD District is the underlying basis for these design guidelines. East Central Avenue corridor is a developed commercial area with a mix of architectural styles. Two-and three story Italianate influenced brick buildings are present, especially along East Central Avenue between William Street and Main Street. Typically, these are first floor retail uses with upper story office or some limited residential uses. Historic architectural elements include the flat front, decorative ornamented cornice, and detailed brick façade. Most buildings along East Central Avenue commercial corridor are built to

¹ Town of Orangetown Comprehensive Plan, May 2003, page V-12.

² Pearl River TOD Opportunities Analysis, Land Use Law Center and Kevin Dwarka, LLC, November 28, 2018, page 46.

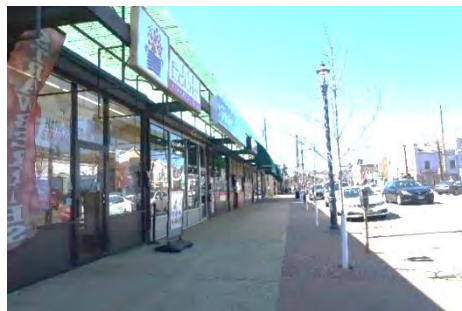
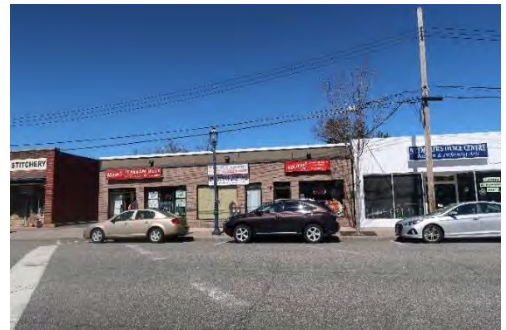
the front and side property lines. Parking, if provided, is located to the rear. The following photos illustrate conditions along the Central Avenue downtown area.



Recent building construction has attempted to replicate the architectural ambience with The Mews, a newer commercial building on E. Central Avenue at William Street.



Many one-story commercial buildings are also present along the commercial corridor. These buildings are typical horizontal “one block” architecture from the early 20th century and later. Evidence of some façade renovations, including new display windows, façade changes, and large wall signs are apparent.



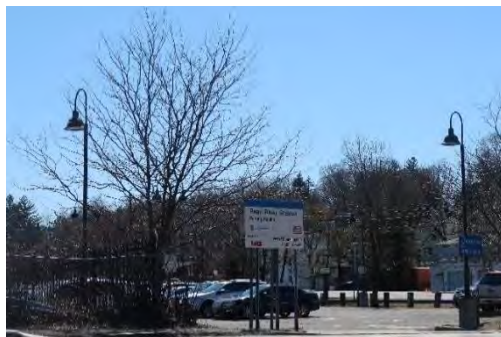
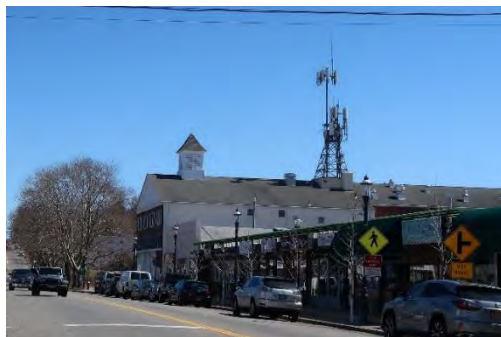
West of the railroad right-of-way is a mix of uses - retail, office, industrial, warehousing, and manufacturing. There are also a number of larger properties, which provide future redevelopment opportunities. Dexter Plaza is a prime example of an older light industrial complex with many users.



There are also special purpose structures such as the Chase Bank, which provide strong design element and counterpoint to the other commercial uses.



Public buildings and sites such as the Fire Station, the train station building (now a cafe), post office, recreation fields, train station parking lots, and Braunsdorf Park across from the train station on Main Street are other design elements within the TOD District.



The Town has undertaken streetscape improvements along E. Central Avenue including decorative street lights, brick paver sidewalks, pedestrian crossings at key intersections, and street trees to upgrade the downtown area. These improvements are referenced in the design guidelines and are a required design feature as development occurs in the TOD district streets.

DESIGN GUIDELINES

The Pearl River Transit Oriented Development ("TOD") District design guidelines support future development in the TOD and build on the historic character of the older buildings along Main Street and Central Avenue which forms the corridor to the Pearl River train station. These guidelines create a framework for future development that will help to create and invigorate a more diverse, active, and engaging Pearl River downtown. They provide guidance for building architecture, signs, circulation, lighting, street furniture, landscape treatment and other features that will help to insure compatible future development that will be integrated within the existing fabric of the Pearl River downtown.

The Pearl River TOD Design Guidelines are adopted as part of the zoning regulations over the 6 Pearl River TOD districts – TOD-MUR₁, TOD-MUR₂, TOD-MUR₃, TOD-OR₁, TOD-OR₂, TOD-OR₃.

Design Guidelines Purpose

The purpose of the Pearl River Hamlet TOD District Design Guidelines is to provide direction to implement the TOD District. The use of the term "**should**" indicates a preferred and desirable standard. The use of the term "**shall**" indicate a mandatory standard. Relief from a mandatory standard may be granted by the Planning Board, unless otherwise specified in this Plan.

Design Guidelines Principles

- The historic character of the TOD District, especially along East Central Avenue, should be celebrated by recycling of the more architecturally significant buildings through adaptive reuse.
- All structures shall be constructed in a compatible architectural theme, with appropriate variations in design. The architectural theme shall include buildings, signage, fencing, lighting, paving, landscaping and other similar and related physical features and amenities.
- All building walls facing any public street, customer parking area or residential district line shall be suitably finished for aesthetic purposes.
- All buildings within any development site shall be compatibly designed, whether constructed all at one time or in stages over a period of time.
- Buildings should to be located at the sidewalk edge with a maximum build-to line continuing the historic streetscape building wall.
- Driveways from Central Avenue and Main Street should be limited, and shared driveway access to parking should be encouraged. Alleys or driveway widths should be minimized, to the greatest extent possible, to limit openings on the street.
- Any driveways and alleys between buildings should be designed to create a pleasant visual, well lit, pedestrian experience.
- Visually pleasing, inviting, and safe pedestrian circulation is required.
- Site improvements should support bicycle use and public transit.

Site Design Principles

- New building shall have architectural features in keeping with these design guidelines that create aesthetic value to their facade walls and the TOD district at large.
- Exterior building elevations must be reviewed for conformance with this requirement and approved as part of the overall site plan review process.
- New construction should be in keeping with the intent of the design guidelines without limiting creativity.
- Streetscape improvements shall replicate the design and materials of the East Central Avenue streetscape improvements including lighting, sidewalk pattern, street furnishing, tree grates, benches, and other amenities.
- Interior site improvements including paving, lighting, benches shall be compatible with this streetscape design palette.
- All sidewalks shall incorporate measures for pedestrian and vehicular safety and shall comply with State and Federally mandated ADA requirements.
- Open space shall be provided with park-like features for shade and passive enjoyment, support for new non-residential activity, play space and flexible gathering and event use.

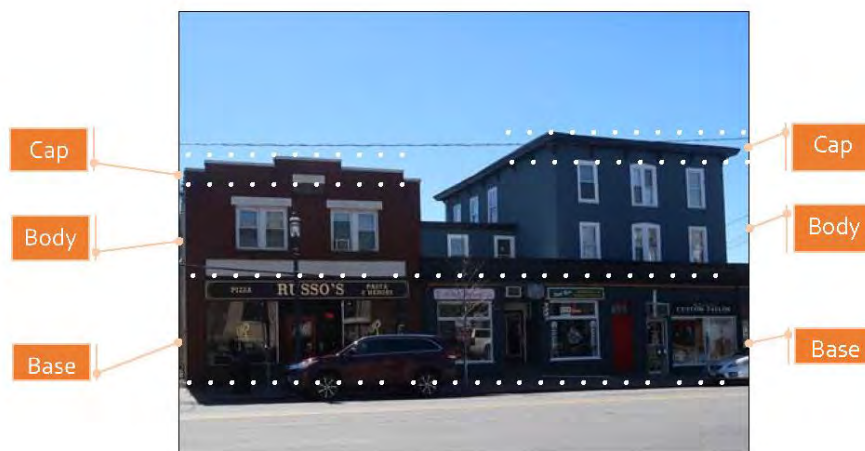
ARCHITECTURAL STANDARDS

Building Materials and Design Articulation

Building facades shall utilize brick, masonry, stone or cast stone or other high-quality materials. Vinyl and wood siding, clapboard and cedar shakes shall not be permitted. Building materials should use natural earth tone colors and should harmonize with surrounding buildings. No more than three primary materials should be used; variations in colors and pattern may be used to break up the mass of the building.

Façade Sections

The building façade shall have a clearly defined base, body, and cap as illustrated in the sketch below.



Architectural Elements

Architectural building elements identified in the Design Guidelines are shown in the illustration below.



BASE

The base of the building should have a more ornate or architecturally significant appearance. A window base (kick plate) should be considered at the ground level to stabilize the building mass consistent with historic commercial building treatments. Base level non-residential uses should include a larger storefront display window encompassing at least 50% of the wall area. Transoms windows should also be considered on the base level above the display window and above the entry door. If the base level has residential uses, as permitted in the TOD-MUR₃ and TOD-OR₃ districts, the windows should be sized appropriately for residential use with a vertical orientation.

Decorative treatment between the base and body of the buildings should include a decorative frieze of wood, stone, or fiberglass above a signboard on mixed use buildings. Details, such as the frieze, should be extended around the corner of a building. Any building façade that faces a public or private street, parking area, public alley or is visible from a street or railroad right-of-way shall use the same materials, building design and have the same quality of appearance as the front façade.

BODY

The body of the building may be less ornate, generally of brick or masonry of a traditional color. The body shall be distinguished from the base and cap by use of horizontal belt courses, changes in material, detailing of the materials, fenestration arrangements and proportion. The windows should be residentially sized with a vertical orientation, whenever possible. The window framing and surrounding material design elements should offer contrast to the surrounding wall. Special treatment of the window lintel, sill, and frame is encouraged to reflect historic building designs.

The body of the building may be horizontally divided at floor, lintel, or sill levels with belt courses. The architectural treatment of a façade shall be completely continued around all street and open yard facing facades of a building. All sides of a building shall be architecturally designed so as to be consistent with regards to style, materials, color and details.

Building wall offsets, including projections and recesses, such as balconies, canopies, awnings, and other architectural details, are encouraged. Building exteriors should have vertical and/or horizontal offsets to create visual breaks on the exterior. Blank building walls of 25 feet in width or greater shall have vertical separations, projecting at least 6 inches to create a visual break. Long, blank, windowless, monotonous, uninterrupted walls or roof planes are not permitted.

CAP

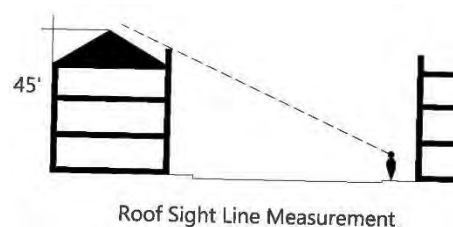
A decorative cornice shall cap the building. It should consist of wood, stone or fiberglass and wrap around the corner of the building on all visible facades.

Roofs

To continue the historic character of the Pearl River TOD, buildings shall be designed to appear as a flat roof terminating with a parapet. This parapet shall project vertically to hide any roof-mounted mechanical equipment. Additionally, a cornice shall project out horizontally from the façade and shall be ornamented with moldings, brackets, or other detailing.



The buildings may have pitched roofs; however, the pitched roof shall not be visible from the public street. The pedestrian sight line across the street from the building should block the view of the pitched roof. See the Roof Sight Line Measurement diagram for details.



The shape, pitch, and color of a roof shall be architecturally compatible with the style, materials, and colors of such building. If the building has a pitched roof, a minimum pitch of 5:12 shall be provided. A

pitch of 8:12 is encouraged. Pitched roofs are encouraged to have dormers, chimneys, cupolas, and other similar elements to provide architectural interest. These elements shall be compatible with the style, materials, colors, and details of the building. Roofline offsets shall be provided along any roof measuring more than 30 feet in length in order to provide architectural interest and articulation to a building.

Rooftop heating, ventilating, and air conditioning (HVAC) systems, exhaust pipes and stacks, satellite dishes, and other telecommunications receiving devices shall be screened or otherwise specially treated to be inconspicuous as viewed from the primary to secondary street and adjacent properties.

In order to reduce the "Heat Island Effect," roofing materials for at least 75% of the roof area of all new buildings should have a Solar Reflectance Index (SRI) of at least 29 for roofs with a slope greater than 2:12 and an SRI of 78 for roofs with a slope of 2:12 or less.

Building Transparency

- Non-residential uses on the ground floor should have large pane display windows along the street frontage at an elevation of between 3 feet and 8 feet above grade.
- At least 50% of the street wall shall be a glass display window. Transom windows above display windows are also encouraged along the ground floor frontage.
- Window sills shall not be more than 3 feet above the sidewalk.
- Decorative window base panels or kick plates, of 18" to 24" in height, are encouraged between the sidewalk and the window sills.
- Glass blocks are not permitted on front windows at street level.
- Exterior security gates are prohibited.
- No blank walls exceeding 25 feet in length shall occur along sidewalks.



Entrances

PRIMARY ENTRANCES

The primary entrance to any building shall front on a major or secondary street. All entrances to a building shall be defined and articulated by architectural elements such as lintels, pediments, pilasters, columns, overhangs, or canopies. These elements shall be compatible with the style and materials of the building. Entrances may also be defined by planters.



CORNER ENTRANCES

Buildings on corners are significant buildings because they have two frontages visible from the street. Corner buildings act as important landmarks within the district and provide visual emphasis. For example, at the intersection of E. Central Avenue and Main Street, the Chase Bank Building accentuates this important intersection. The corner is also further defined by an attractive water fountain, flag pole and landscaping. The building on Central Avenue west of the railroad (shown below) has been attractively renovated. The turret forms an important building design element at the corner.



At street intersections, the corners of buildings can be emphasized with special entrance treatments. Special design elements at corners entrances such as clock towers, cupolas, truncated walls cut diagonally with corner door entries, contrasting façade materials can be used at key corners to increase visibility at the street intersection and provide a strong design statement.



Awnings

Awnings are encouraged above retail storefronts and over major building entrances. They are permitted above windows and doors. Extended awnings should be in the shed style in the shape of a sloped flat plane. Internally illuminated awnings are not permitted. No awning shall extend more than 3 ½ feet from the building exterior or be less than 8 feet from the sidewalk grade level. Awning valances should be not more than 10 inches in height. Signs can be included only on the awning valance.



Balconies

Upper level balconies are permitted on all residential facades. Balconies should be fully recessed within the building plane. However French balconies (shallow balconies connected to French doors, typically of wrought iron) are permitted to project from the building façade up to 30 inches.



SIGNAGE

Guiding Principles

Signs should reflect the scale of the building and not obscure the building architectural features. Well-designed signs will add variety and visual interest to the streetscape. A sign plan shall be provided as part of the site plan application. New signage shall be fabricated in materials and styles compatible to the character of the buildings subject to the standards below. Deviations from the signage may be permitted to address the size and scale of buildings and to address project-wide signage that may be developed on larger projects, especially in the TOD-MUR₃ and TOD-OR₃ districts. Signage shall reinforce pedestrian scale. Sign clutter shall be avoided.

Prohibited Sign Features

- No sign shall have flashing lights or exposed high-intensity illumination or reflective material or be of neon letters.

- No sign shall obstruct any window, door, fire escape, stairway or opening intended to provide light or ingress or egress to or from any building or structure.
- Exterior signs using moving parts or banners, spinners, flags and pennant, with the exception of clocks and weather information.
- No plexiglass type box signs, that may be internally lighted, are not allowed.

Business Signage

Each business is permitted the following signs: **wall, hanging, awning /canopy, and window signs.**

WALL SIGNS

Walls signs are most appropriately located in the frieze above the storefront display and transom windows for individual businesses. Wall signs shall be a maximum area of 30 square feet and must be located below the second story floor line within the frieze/sign board area. Wall signage is also permitted for walls facing rear parking areas with the same sign area as permitted on the front façade. The wall signs shall be either the following materials:

- Internally-lit raised letters with concealed ballast.
- Back-lit raised letters of wood or metal with concealed ballast.
- A wood or fiberglass sign board or separate wood or metal letters with overhead gooseneck type lighting

The sign graphics or logos should be at least 8 inches to be readable. They should not be more than 12" letters. Raised free standing letters should not be more than 12 inches from the building wall.



HANGING SIGNS

- One hanging sign per each business is permitted. The maximum sign area shall be 10 square feet. Letter/logo height shall be a maximum of 8 inches.
- Hanging signs shall be located below the second story floor line, with a lower edge clearance from the ground level of a minimum of 8 feet.
- Sign shall be constructed of wood or metal. They shall not be internally lighted.
- Signs must be rigid.
- Plastic, vacuum formed internally illuminated signs are not appropriate.



AWNING SIGNS

One sign with lettering per business shall be permitted on an awning only on the base floor. Sign shall be located on the vertical flap of the shed style awning. The height of the letters shall not exceed 8 inches. In cases of multi-tenanted buildings, all awnings shall be of compatible color, size, and style. Awnings shall be kept in good order and repair. They shall be made of a weather resistant cloth or canvas. There shall not be internal lighting under the awning.



WINDOW SIGNS

All window lettering and logo signs shall be inside on the display or transom windows and only on the first floor windows. Window lettering/logo signs shall not exceed 10% of the window area. Internal hanging signs shall not exceed 15% of the window area. Lettering height shall not exceed 8 inches. These signs shall pertain only to the business occupying the premises where the window is located.



Monument Signs

One monument sign shall be permitted to identify developments in the TOD-MUR3 and TOD-OR3 zones only. The total area of the monument sign, including sign face and support structure shall be limited to 30 square feet. The height of the sign shall not exceed a height of six feet above the grade. The monument sign shall be setback a minimum of 10 feet from any property line. A double faced sign shall be counted as one sign. The monument sign materials shall be compatible with the building design treatment of quality materials. The monument sign shall not be internally lighted.



Street Address Signs

A street address sign is required for each building or individual tenant. Street numbers shall be a maximum of 8" in height.

Temporary Signs

Temporary signs advertising the sale, rental and development of a site shall be permitted provided:

- No more than one such sign shall be permitted per street frontage.
- No one surface of any such sign shall exceed 24 square feet in area.
- No such sign shall exceed six feet in height.
- No such sign shall be located less than 10 feet from any lot line.
- No such sign shall be illuminated in any manner.

Directory Signs

Multi-tenanted commercial buildings may provide a directory sign on the main entrance to the upper floors of the building. These directory signs are permitted only for upper story non-residential uses. The directory sign shall be a wall sign not to exceed 15 square feet in area. The sign shall not be internally lighted. The letter height shall not exceed 4".



Wayfinding Signage

Wayfinding signage is an important streetscape element. It aids visitors and also acts as an additional aesthetic visual element in the streetscape. A wayfinding signage plan for the Pearl River TOD District should be developed by the Town of Orangetown. It shall provide information identifying the location of the train station, public parking lots, businesses and public buildings.



CIRCULATION

Driveways

Driveways into the rear parking areas shall be limited to 20' wide driveway openings. Where possible, driveways openings shall not be located on East Central Avenue or Main Street. Shared driveway access to rear parking areas between building lots is highly encouraged. Parking areas should be connected between properties to facilitate parking access and utilization.

Parking

Parking requirements for all permitted uses are stated in the TOD district regulations. Parking should be designed to meet the following standards:

- On-site surface parking shall not be located between a principal building and any street frontage.
- Off-street parking areas should be located to the rear of the building groups, landscaped and screened.
- Shared parking areas and shared driveway access to the rear of lots is strongly encouraged.
- Shared maintenance and cross-easements between adjoining properties shall be implemented by legal documentation to be reviewed and approved by the Town Planning Board Attorney.
- Parking spaces developed on the ground level under buildings should include commercial liner buildings along the street frontage to maintain pedestrian activity.

Walkways

Walkways between buildings shall be designed to create an attractive entry experience to side and rear entrances and parking areas. Trellises, archway, landscaping treatments can be used to create an aesthetic walking experience along the walkways and between buildings.



Parking Structures

Parking structures shall be designed to fit within the building streetscape. Parking structures shall meet the building setback and other bulk regulations and the exterior architecture treatment shall be compatible with the building design guidelines. Parking structures should be designed to include retail liner buildings along the street frontage to maintain the streetscape rhythm. Wherever possible, vehicle access should be from the side street or service drive and visually screened from view from the public street or walkway.



Source of garage photos: <http://plannersweb.com/2014/12/fbc3/parking-garage-liner-spaces-staunton>
<http://online.encodeplus.com/regs/overlandpark-ks>

Traffic Calming

Textured paver crosswalks shall be used in place of traditional painted lines for street improvements. The use of curb extensions on new internal roads and drives in the TOD-MUR₃ and TOD-OR₃ Districts should be used to improve pedestrian safety.

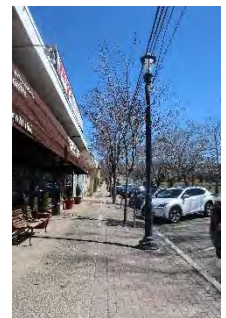
Pedestrian/Bike System

Pedestrian sidewalks shall be provided to encourage and support walkability. The pavement design should be consistent with the Pearl River TOD sidewalk improvement detail. Shared bike lanes should be considered and marked accordingly where appropriate. Bike racks should be included in the site amenities design.

LIGHTING

Street Lights

Street lights shall be the fixture type selected by the Town for the TOD district or an approved compatible design in terms of design and color. Pedestrian scaled lighting, less than 16 feet in height, shall be used along all internal streets within new developments. An average of 0.2 foot-candle illumination should be maintained over the sidewalks. Street light fixtures shall be full cut-off.



Parking Lot Lighting

Parking lot lighting fixtures shall be of a design compatible with the TOD district street light fixture.

- Fixture poles shall be no more than 25 feet in height.
- An average of 0.8 foot candle illumination must be maintained within residential parking lots.
- An average of 2.6 foot candle illumination must be maintained within non-residential parking lots.
- Parking lot fixtures shall be full cut-off and shall be on timers.

Building Lighting

Light fixtures attached to the exterior of a building are encouraged. These fixtures shall be architecturally compatible with the style, material and colors of such building.

BUFFER AREAS

Buffer areas shall be provided at a minimum of 25' wide where properties adjoin the residential zone. These buffer areas shall be developed in an aesthetic manner for the primary purpose of providing a year-round visual screen to the development from adjacent rights-of-way and land uses. The buffer shall contain dense plantings of evergreen trees, deciduous trees, flowering trees and shrubs. Structures and parking shall not be permitted in the required buffer area; however, fences and walls are permitted in the buffer area.



UTILITIES

New site development and complete reconstruction of a property shall place all utilities underground, unless specifically excluded by the Town Engineer.

STREET FURNITURE

Street furniture should be provided as part of the site improvements. These design elements shall be compatible with the Pearl River TOD streetscape design elements. The following presents examples compatible street furniture in keeping with the historic character of the District.

BENCHES

Benches encourage social interaction and add eyes on the street, making it a safer place. It is preferable to locate benches close to street trees so that they will benefit from summertime shading.



TRASH RECEPTACLES

Trash receptacles are important in keeping the sidewalks clean. They can also encourage recycling and reuse.

BIKE RACKS

The installation of bike racks encourages an alternative transportation mode. Bike racks provide a safe and appropriate location for bicycle storage and prevent the destruction that occurs when bikes are chained to street lamps and trees.



NEWSPAPER STANDS

Newspaper stands should be grouped to decrease cluttering of the sidewalk. Newspaper stand enclosures should be used in accord with Town standards.

KIOSKS

Kiosks should be provided in the public spaces to provide space for posting of notices and activities.



PLANTERS

Planters provide the opportunity for smaller scaled vegetation along the sidewalk. They are highly recommended to be used to define shop entrances, enclose outdoor dining areas and embellish the spaces between street trees.



TABLES

Tables and umbrellas provide space for outdoor dining and viewing the street activity. The style should be consistent with the design palette of the streetscape. Black wrought iron type is recommended.



PUBLIC ART

Murals and other public art can provide additional street vibrancy and visual interest in the District. Any mural or art provided along the street or located on building walls within the district shall be subject to Planning Board review and approval.



LANDSCAPE ELEMENTS

Landscape Standards

Street trees are an essential element of a streetscape; they provide shade to pedestrians in the summer months, improve air quality and provide natural beauty in a developed environment. Each street typology states whether trees are planted in a tree grate or planting strip. The following planting standards apply:

- All trees shall be planted at least 2.5' from the curb face
- Trees to be planted in planting beds shall be planted in a bed measuring a minimum of 4' x 8'
- Trees shall be supplemented by native, low-maintenance plants
- Trees to be planted in grates shall be planted in a grate measuring a minimum of 5' by 5'
- Trees to be planted in grates shall be planted in structural soil. Structural soil material is designed as follows. The three components of the structural soil are mixed in the following properties by weight: crushed stone: 100; clay loam: 20; hydrogel: 0.03. Total moisture at mixing should be 10% (AASHTO T-99 optimum moisture).
- Trees shall be planted at a minimum size of 2.5 to 3 inch caliper, bound and burlapped.

Guiding Principles

The landscape plan should conform to the following general design principles:

- All landscape plants should be native species and typical full specimens conforming to the American Association of Nurserymen Standards (ANA) for quality and installation.
- All plant selections should emphasize deer resistant species.
- Local soil conditions and water availability should be considered in the plant selection. All plants shall be tolerant of specific site conditions.
- Landscaping shall not inhibit access by emergency vehicles or inhibit visibility within required vehicular sight triangles.

- Only irrigation systems using nonpotable water supplies are to be used for all new plantings.
- An appropriate variety of tree species should be provided to avoid die-out due to species-specific diseases.

Street Tree Details

Street trees should be provided in accordance with the following standards:

- Street trees should be provided on both sides of all streets
- The number of street trees should average one for every 35 linear feet of property frontage.
- Spacing between trees should be determined based upon species selection. In general, trees should be between 30 and 50 feet on center, averaging no more than 40 feet on center (excluding driveways and utility vaults).
- Trees should be a minimum of two and one-half to three inches caliper, based on ANA standards.
- Trees are to be disease resistant and tolerant of road salts and air pollution.
- Branching height should bear a relationship to the size and species of tree, but shall have a minimum clearance height of seven feet above grade before branching begins.
- Along narrow street frontages, columnar and/or fastigate trees should be specified to accommodate canopy width.

Parking Lot Landscaping

Interior parking lot landscaping shall be provided as follows:

- 4 percent of the gross square footage of paved parking area shall be landscaped.
- Landscaping shall be provided in areas of at least 150 square feet.
- A minimum of 1 shade tree shall be planted within the parking area for every 10 parking spaces.
- Shrubbery shall be less than 3 feet and shade trees shall have foliage no lower than 7 feet in height to provide for safe visibility.
- No row of parking spaces shall exceed 20 spaces without interruption by a minimum 9 foot wide landscaped island.
- Plants shall be selected for high drought tolerance or irrigation shall be provided that uses only nonpotable water supplies such as captured rainwater or greywater.
- All tree plantings within landscaped islands shall be set back 2 feet from the curb.

Landscape Buffer Details

A landscaped buffer shall be provided around surface parking lots and around the perimeter of new development sites in accordance with the following:

- A shrub mass of deciduous and/or evergreen species shall be planted within the required buffer area to provide for a visual and physical screen along the entire frontage.
- Shrubs within the buffer shall primarily include evergreen species, although deciduous plants may be used provided that their use does not result in significant visual openings during the winter season. This landscape mass shall be interspersed with the required ornamental and evergreen trees to provide for a natural, random and visually interesting plant scheme.

- Selection of plants species shall provide for a variety and mixture of landscaping. Varieties should consider susceptibility to disease, shapes, seasonal display, textures, flowers, and foliage.
- The plant quantities constituting the buffer should include:
 - Shrubs averaging 25 per 100 linear feet of frontage.
 - Ornamental trees averaging four per 100 linear feet of frontage.
 - Evergreen trees averaging four per 100 linear feet of frontage.
 - Lawn or groundcover to complete the required buffer area.
- Required plantings in the buffer area shall meet the minimum size requirements as follows:
 - Shrubs: planted size is to be a minimum of 24 inches to 36 inches in height.
 - Ornamental trees: planted size is to be a minimum of six feet to eight feet in height.
 - Evergreen trees: planted size is to be a minimum of five feet to six feet in height.

Planting Specifications

- Deciduous trees shall have a minimum 2 1/2 to 3 inch caliper measured 6 inches above the ground surface at time of installation. Size of evergreens and deciduous shrubs shall be allowed to vary depending on setting and type of shrub. Only nursery-grown plant materials shall be acceptable horticultural standards.
- Dead or dying plants shall be replaced during the following planting season and guaranteed by the landscape contractor for a period of one year.
- Evergreen trees shall have a minimum height of 6 feet at planting. All plant materials, planting practices, and specifications shall be in accordance with the "American Standards for Nursery Stock" by the American Association of Nurserymen Standards.
- The plant species selected should be hardy for the particular climatic zone in which the development is located and appropriate in terms of function and size.
- Landscaping within sight triangles shall not exceed a mature height of 30 inches and shade trees shall be pruned up to a ten (10) foot branching height above grade.
- Any area not used for buildings, structures or paved areas shall be planted with an all-season ground cover or lawn and other landscape materials including, but not limited to, trees, shrubs, berms, plants, etc., as needed to meet the requirements for adequate landscaping and buffering as approved by the Planning Board.
- A landscape plan shall be prepared by a landscape architect, licensed by the New York State Board of Landscape Architects.

SITE DESIGN STANDARDS - TOD-MUR₃ AND TOD-OR₃ DISTRICT PROPERTIES

Site Design

Building architecture should be compatible with the historic character of commercial/mixed use buildings in the TOD zone. Large building sites within these districts should have internal streets and walkways designed to replicate a mixed-use street like East Central Avenue. The buildings should be located within the build-to-line close to the street, and on-street parallel parking should be provided. The sidewalk realm should include pedestrian amenities to encourage street level activity. Sidewalk connections shall be provided linking to public streets and connection to the Pearl River train station.

Parking

On-street parking shall be provided as part of a comprehensive streetscape design approach to new developments within the TOD- MUR₃ and TOD-OR₃ districts. Off-street parking areas should be located to the rear of the building groups, landscaped and screened. Open parking areas along public and internal streets should be discouraged.

Open Space

TOD-MUR₃ and TOD- OR₃ development shall provide recreation playground areas as part of any planned mixed use/residential development. These recreation areas shall provide facilities for children of all age groups in separate locations. Seating, trash receptacles, lighting, landscaping shall be provided in an integrated design plan.

Building Setbacks

TOD-MUR₃ and TOD-OR₃ development shall provide the following minimum setbacks between multiple buildings located internal to the lots shall be as follows:

- Front wall to front wall 75 feet
- Front wall to rear wall 75 feet
- Rear wall to rear wall 75 feet
- Side wall to side wall 50 feet
- Side wall to front or rear wall 50 feet
- Building setback from roads and drives - 0' Minimum 15' maximum measured from the interior edge of the sidewalk.

Other Standards

All utilities within the TOD-MUR₃, and TOD-OR₃ districts shall be placed underground.

The Muddy Creek shall be protected and designed as a water-related amenity for new development in the TOD-MUR₃ and TOD -OR₃ districts.

Other design standards shall be in effect unless superseded by this section.

SUSTAINABILITY

Guiding Principles

Encourage environmentally conscious strategies to reduce the consumption of resources and support municipal policies for sustainability.

Provide new construction for buildings and structures that advance high performance reduction in energy use and address sustainability in all aspects of design.

Requirements

- Building and site design should seek to minimize environmental damage, reduce energy and water use, promote sustainable, renewable, non-toxic and locally-produced materials, improve indoor air quality, and provide environmentally conscious planning.
- New construction should incorporate the use of salvaged materials, recycled materials, regionally produced materials, renewable materials, materials with low or no VOCs in accordance with referenced standards.
- Design should incorporate appropriate areas for collection and storage of recyclable materials and refuse.
- Parking garage areas shall accommodate alternate energy vehicles and secure bicycle racks. Bicycle racks should also be located in open site areas convenient to primary entries.
- Green street technology should be incorporated to all street improvements to aid infiltration of excess stormwater. These green building design goals should be encouraged. Formal LEED certification is encouraged, but not required.

APPENDIX

TOD DISTRICT GATEWAYS

The East Central Avenue and Pearl Street (Route 304) intersection provides an opportunity to create a major gateway area into the Pearl Hamlet TOD District. Addressing this intersection in the site design can emphasize this special entry area. Similarly, the East Jefferson Avenue and South Pearl Street, and East Washington Ave and North Pearl Street intersection should be considered secondary gateways into the Pearl Street TOD District with special streetscape and intersection treatment. East Central Avenue and John Street is the eastern gateway into the TOD which can also be emphasized through signage, landscape treatment and future building design improvements.

The following sketches illustrate conceptual ideas for the East Central Avenue/Route 304 gateway view east.



Street level view facing east towards downtown on East Central Avenue at Route 304.



Aerial view of East Central Avenue and Route 304 intersection.



Proposed site plan of intersection over existing aerial photo. The Saloon Bar and other buildings on the northside, except the corner property, are shown to remain. New mixed use development is assumed on the Dexter Plaza property. The site plan sketch only looks at the Central Avenue/ Route 304 frontage at this intersection. Future development would occur beyond this frontage.



A pedestrian walkway provides access between the buildings to rear. Decorative pavement, lighting fixtures, trees and other streetscape materials are provided. Parallel parking is maintained along East Central Avenue.



*The buildings are sited on the build-to-line, close to the street frontage. A decorative "Welcome to Pearl River" gateway sign is installed. **The design of the gateway sign and other public improvements should be cognizant of and reflect the history of Pearl River.** The parking on Central Avenue is modified to widen the sidewalk on northside of street in front of the Saloon Bar and other properties. Curb bump-outs provide a safer pedestrian crossing distance. The streetscape has brick pavers, decorative lighting planters, benches and other street furniture.*



Special entrance treatments are provided to highlight buildings along corners. Curb bump-outs provide a safer crossing at the intersection. Parallel parking is provided on both sides of East Central Avenue. Buildings are sited at the build-to-line, establishing a continuous frontage building façade treatment. The design of public improvements, such as the gateway sign, should reflect the history of Pearl River.

CONCEPT SKETCHES ILLUSTRATING THE TOD DESIGN GUIDELINES

The following concept sketches illustrate possible redevelopment opportunities utilizing the TOD Design Guidelines.



Existing Condition – East Central Avenue at Municipal Parking Lot Entrance







*View looking east on East Central Avenue toward John Street
and the Fire Station.*





EXAMPLES OF ARCHITECTURAL TREATMENT COMPATIBLE WITH DESIGN GUIDELINES

Mixed Use Buildings – 2 story



Mixed Use Buildings – 3 story



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Residential Buildings – 3 story



Mixed-Use Buildings – 2 & 3 story with garage parking

